



Downtown Plan

Appendix 'G'
To Bylaw No. 1983-2011

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1.0 Introduction

Downtown is the heart of a community, often containing an interesting mix of modern and historical buildings, public spaces and parks, and important every day activities and functions. An active downtown provides a showcase for the community's vision and future. Downtown Terrace is no exception, as it is an important focal point for activity within the community. Within the downtown area a diversity of uses can be found such as City Hall and other municipal and institutional buildings, parks and recreation facilities, residential uses, and a variety of commercial uses including office, street level retail, hotels, and restaurants.

Improvements to the downtown streetscape foster revitalization and encourage a more lively and active downtown. Commercial areas, especially the downtown, represent a major tax base and a significant employment generator. They also play host to many social, cultural, and recreational activities, such as the Farmers Market and Riverboat Days. When all is said and done, the downtown is an essential community and tourism resource. While the existing elements provide a strong foundation for the downtown, City Council along with the Downtown Merchants and the wider

community have identified the need to improve and emphasize the downtown area.

In 2006, the City of Terrace initiated the development of a downtown revitalization planning process. Council identified that an inviting and attractive downtown represents a significant component in creating a healthy and vibrant community. It is intended that the ***Downtown Plan*** be adopted by Council as a schedule to the Official Community Plan (OCP #1771-2002). The Plan will build on the vision and the goals for the downtown in the OCP. The ***Downtown Plan*** will supplement the existing Downtown Design Guidelines. Any developments, redevelopments, or improvements in the downtown should be consistent with both the ***Downtown Plan*** and the Downtown Design Guidelines (Schedule 'E' of the OCP).

Recent signs of economic recovery and the positive atmosphere within the community promoted a desire to see change and improvement that will attract residents, local investors, and visitors to the downtown. Staff and downtown merchants met and discussed the various issues facing the downtown. Staff recommended that a Downtown Revitalization Plan be prepared in order to provide direction to achieve

Council's goal of developing an inviting downtown image.

The ***Downtown Plan*** has evolved from a review and analysis of:

- Existing city bylaws and documents pertaining to the downtown.
- Relevant planning and policy documents from other municipalities of similar size in Northern BC.
- Current downtown conditions.
- Comments received from the public.
- Consultation with key downtown stakeholders.

The City has also initiated community discussion concerning problems with vandalism and the future development of the Co-op Lands. The community and stakeholder consultation, review of relevant policies, previous downtown studies and plans together form the basis for the new ***Downtown Plan***.

1.1 Background

As part of the effort to revitalize sections of the downtown, the City has authorized a number of related projects over the past decade.

The December 1998 Evaluation and Assessment of the Downtown prepared by Urban Systems Limited provided a basis for many of the initiated improvements and work that followed.

Council's subsequent adoption of an Official Community Plan Amendment Bylaw for the downtown put into policy and bylaw many of the recommendations contained in the Urban Systems report.

The amendment to the OCP created land use subcategories and development permit guidelines for all properties within the subcategories. Amendment to the Subdivision and Development Bylaw established off-site standards for the land use subcategories created by the OCP amendment.

In addition, the City of Terrace Engineering and Planning Departments initiated a Streetscape Improvement Study in the fall of 2000. The study incorporated the recommendations of the Urban Systems report and the Official Community Plan amendment and resulted in the development

of a street design for the downtown that includes a new street lighting plan, sidewalk bulbs, sidewalk treatment, street furniture (benches, bicycle racks, and waste receptacles), street trees including grates and guards and gateway entry features to the downtown. Although the Streetscape Improvement Plan was not officially adopted, many of the guidelines and directions from the draft plan have been implemented in subsequent downtown street reconstruction projects in recent years.

In 1998, the Downtown Design Guidelines were adopted, providing direction for new development to ensure appropriate and high quality design.

The OCP (2002) provides a comprehensive Vision for Terrace as well as specific objectives for the downtown. The OCP reinforces the importance of maintaining downtown Terrace's viability through encouraging specific types of development in the downtown core. The OCP provides for mixed use development incorporating high quality streetscapes, building design, and landscaping.

In May 2006, the City revisited the downtown studies and conducted public and stakeholder consultation. The scope of the ***Downtown Plan*** was expanded to include direction on social issues, as well as the

development of an implementation strategy. It was recognized that to successfully revitalize the downtown required partnering between local government, property owners, various stakeholders (such as the downtown merchants), and community residents. To achieve a Plan with widespread support required a broad community vision for the entire downtown addressing both the public and private realms.

1.2 Purpose

The City of Terrace ***Downtown Plan*** will provide a comprehensive vision for the downtown area. The scope of this document includes an overall vision and goals for the downtown while including more detailed directions on streetscape, gateways, downtown theme, building and property improvements, signage, and parking and traffic. The purpose of the plan is to provide a comprehensive approach to enhance and revitalize downtown Terrace through multiple and synchronized strategies, including streetscape improvements, gateway enhancements, business improvement areas, and encouragement of private property improvements.

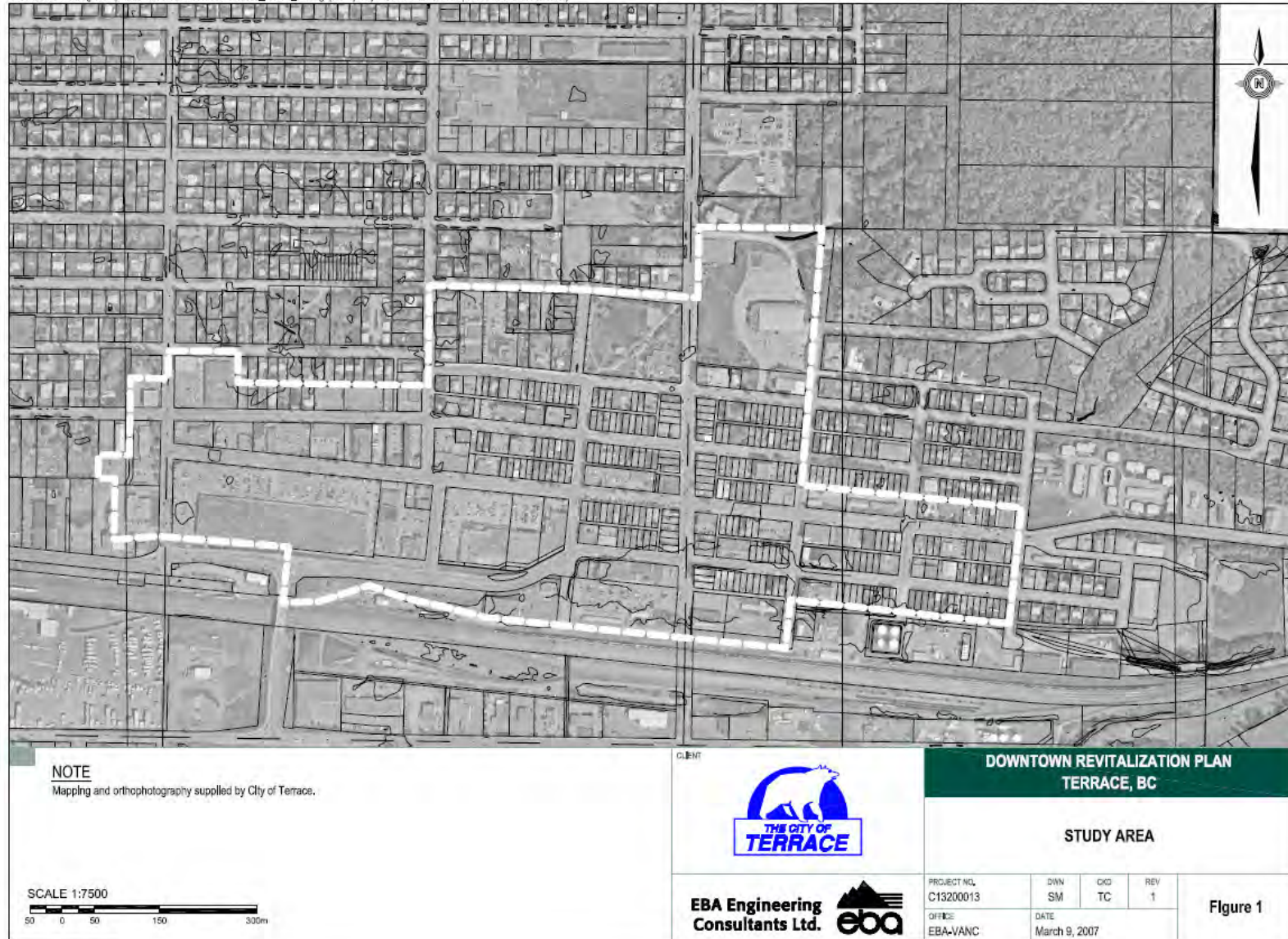
1.3 Study Area

The study area for the plan is generally bounded by Apsley Street at the east, the Railway tracks to the south, Eby Street to the west and Park and Davis Avenues, as well as Upper and Lower Little Parks, to the north (Figure 1).

While the plan primarily focuses on the core commercial areas of Terrace's downtown, the study area expands the scope of previous downtown studies to include the Upper and Lower Little Parks, municipal recreation facilities, City Hall, as well as residential development. While not all the directives of the plan apply to the entire study area, the expanded scope provides for a more comprehensive approach to downtown planning.

DOWNTOWN PLAN

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2.0 Downtown Vision and Goals

The City of Terrace ***Downtown Plan*** provides direction and guidance based on the on-going initiatives in the downtown, as well as stakeholder and public consultation. Public consultation events were held during the summer of 2006, including an Open House on June 29, 2006, a survey, presentations to local organizations and stakeholder meetings, such as the Chamber of Commerce, Rotary Club, and Downtown Merchants Association. The public consultation process provided information and solicited feedback from the public. The presentation reaffirmed the Downtown objectives contained in the Official Community Plan and provided the basis for crafting a Downtown Vision Statement.

2.1 Vision Statement

The Vision Statement summarizes the overarching goal of the ***Downtown Plan*** and provides direction for policy decisions in regards to the downtown.

The City strives to develop a diverse and vibrant downtown that inspires and enriches the experience of local residents, also making it an attractive space for visitors to enjoy. A unique sense of place, reflecting a rich heritage, will foster pride and connection to the downtown.

The Vision Statement embodies the OCP objectives and reflects community input clarifying what they would like their downtown to become. In addition to the Vision Statement, several principles for the downtown were also developed, as outlined below:

Access and Accessibility – There should be equal opportunity for people of all ages and mobility levels to be engaged in the downtown. All elements of the downtown should encourage diverse groups to feel welcome and enjoy themselves.

Strive for Excellence – Providing the opportunity for different uses, activities and spaces to develop as vibrant, complementary, and inspiring places.

Innovative and Creative – Opportunities to promote innovative ideas and creative developments should be embraced. These include supporting mixed use developments, creating a walkable and accessible streetscape that provides important linkages, and enhancing the safety and enjoyability of downtown through traffic calming and Crime Prevention Through Environmental Design (CPTED) principles (see Section 3.6.2 and Appendix A).

Community Connection – Integration, coordination, networking, and partnerships with different interest groups and disciplines will help create stronger ties with the community and increase their personal stake in the downtown. A business improvement area would encourage support and involvement from local businesses.

2.2 Objectives

The central objectives for the ***Downtown Plan*** are the same as those identified in the 2002 OCP. The OCP states the following objectives for the downtown:

1. Establish Downtown Terrace as a desirable place to shop, live, work, and visit.
2. Create a thriving economic centre for the City and the surrounding region.
3. Create a sense of place visually and functionally through shared and coordinated approaches to streetscape, building and landscape design.
4. Encourage partnerships between business, government and private developers that achieve community goals through innovative and creative means.
5. Encourage redevelopment and investment in the existing downtown shopping malls.
6. Create commercial services and cultural activities that cater to all tourists regardless of duration of stay, market group, or ethnic background
7. Preserve and enhance the cultural, historical and natural environment of the downtown.

3.0 Downtown Concept Plan

3.1 Overall Concept

Downtown Terrace has a grid street pattern, oriented parallel to the railway that reflects the city's early history and town layout along the rail line. George Little, the founder of Terrace, established a sawmill in Terrace to provide railway ties, as well as donating 47 acres of his land, for the Grand Trunk Pacific Railway. While riverboats on the Skeena River were widely used prior to 1912, the railroad soon became the most efficient means of transportation to Terrace from Prince George or Prince Rupert. The George Little House, a community heritage resource located at the base of Kalum Street, now acts as the Via Rail station and ties together the importance of George Little and the rail line in Terrace's history.

The rail tracks follow along the north side of the Skeena River and border the south side of downtown. The 1912 survey map shows the grid street pattern established based on the railway location (Figure 2). Historic lot sizes ranged from 33 to 55 feet wide and 85 to 100 feet long.

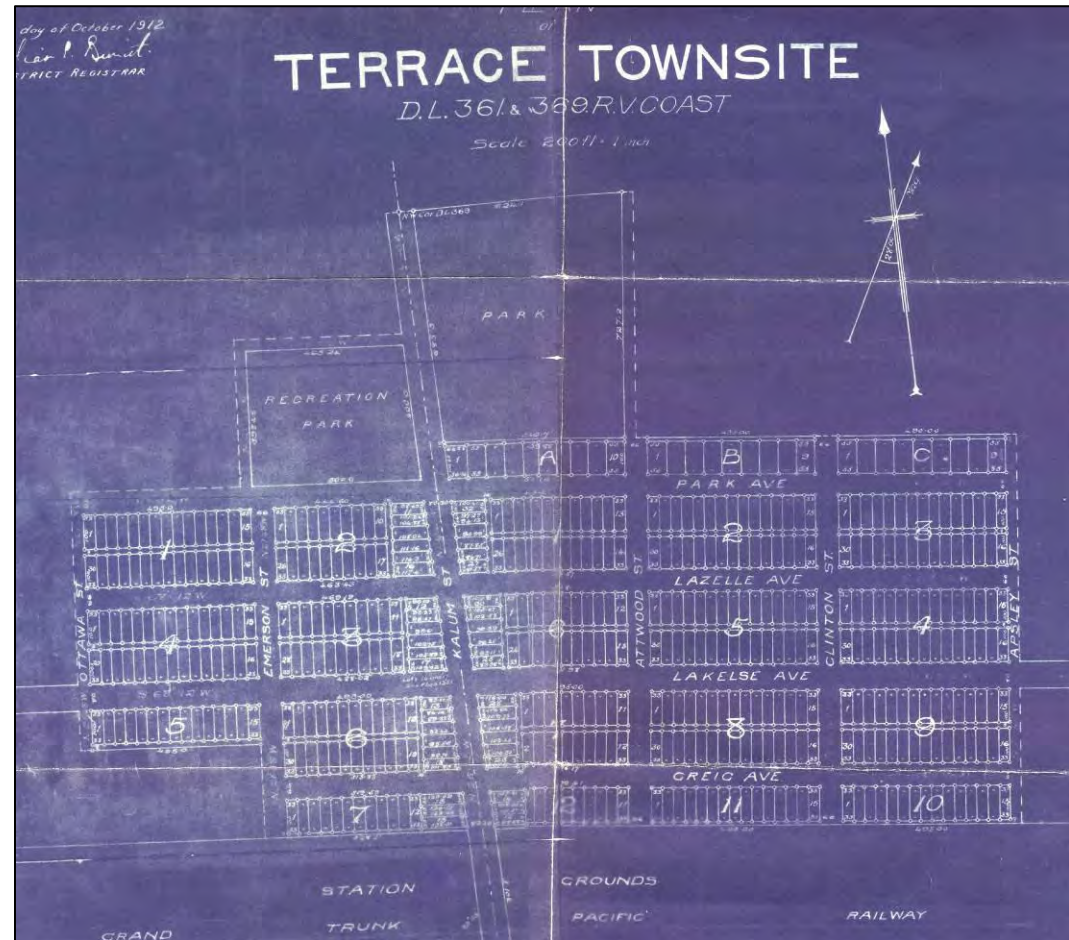


Figure 2 -1912 Terrace Survey Map

DOWNTOWN PLAN

Currently the downtown retains the historic street pattern, but the historic lot sizes and building sizes have changed significantly in some areas through consolidating smaller lots into larger lots. Figure 3 shows the current built-up areas in the downtown. The number of significantly larger buildings, as well as surface parking and vacant lots can be identified. The process of lot consolidation has created a less contiguous downtown environment. Larger buildings affect the permeability of the downtown, typically making it less walkable and the street environment less comfortable.

The vacant lots vary in size and many vacant parcels have been fenced off, also making the downtown less permeable, as well as negatively impacting the overall aesthetic.

Redevelopment in the downtown should consider the historic lot sizes as well as the context of the historic street pattern. While it is not necessary to constrain new building sizes, multiple entryway and detailed architectural façade treatment should be considered to provide interest to the buildings and enhance the pedestrian experience. This can be achieved by breaking up large solid walls with different treatments on the building, such as differences in the materials used, outcroppings or setbacks, and differences in

building details such as awnings, trimmings, recessed or projected windows, and use of color. Providing multiple doorways and large transparent display windows also helps provide visual interest at the street level. The Downtown Design Guidelines in the OCP currently require large buildings to provide a different façade treatment every 10 m or less.

Redevelopment of vacant lots should be encouraged but could be difficult due to factors such as relatively low land demand and in some cases the costs associated with remediation of former gas station sites. Interim solutions to the address vacant lots, former gas station sites in particular, should be investigated. Possibilities include: removing fencing, planting vacant lots with grasses, wildflowers, and other species that would involve minimal maintenance costs, or temporarily placing public art on the site.

Directives

1. The historic street pattern of downtown shall be maintained.
2. Links between important sites/buildings shall be enhanced.
3. Developers shall follow the Downtown Design Guidelines and shall be required to adopt

multiple entryway and detailed architectural façade treatment on new or redeveloped downtown buildings.

4. The City of Terrace should investigate interim solutions for improvements to vacant lots, and contaminated gas station properties.

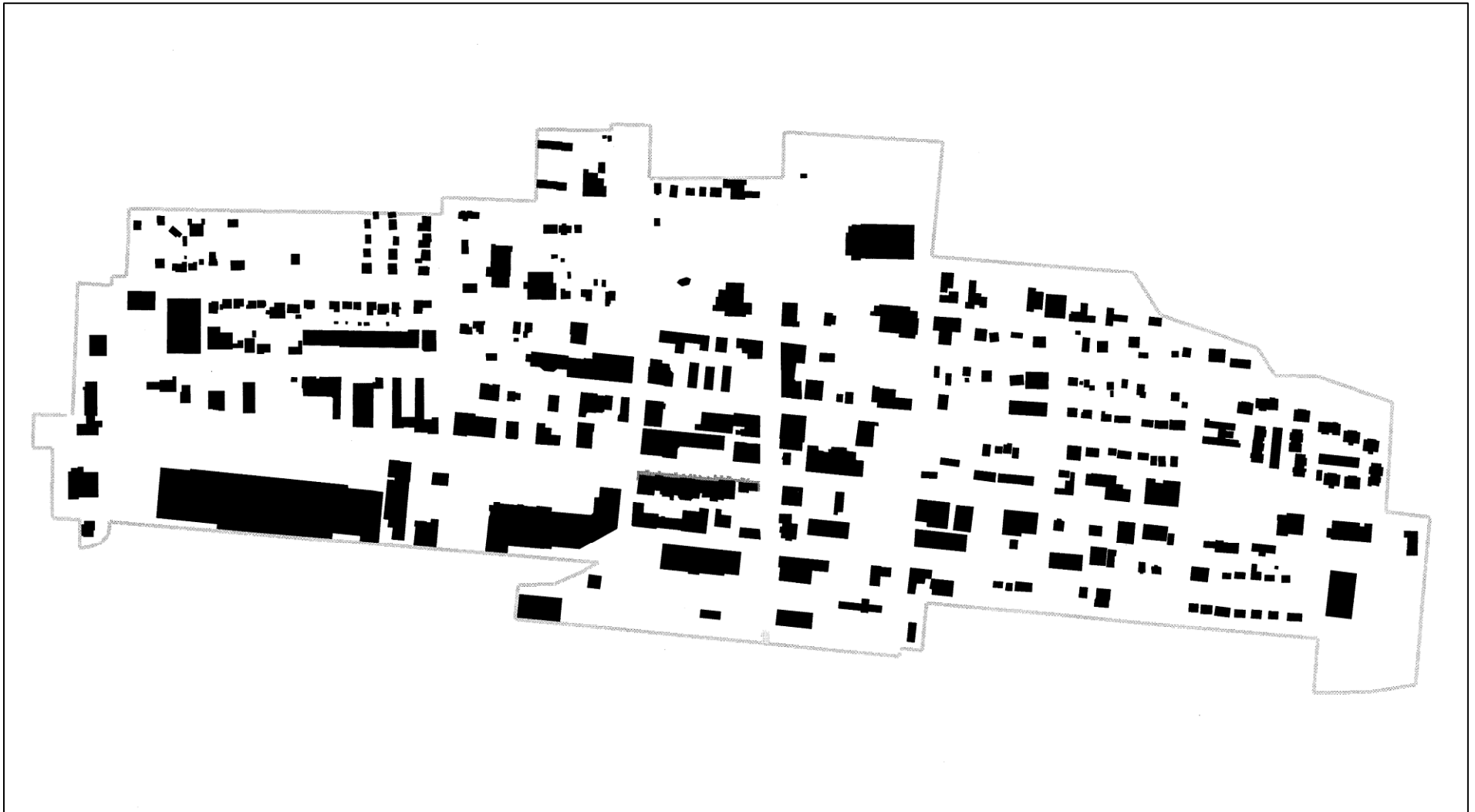


Figure 3 – Built up areas in downtown terrace.
Credit: City of Terrace Planning Department, 2005.

3.2 Theme

A theme can be defined as a distinct, recurring and unifying quality or idea. In the context of the downtown, a theme provides visual cohesiveness to the area. This is accomplished through commonalities in the architectural design of buildings, unifying elements of the streetscape, such as signage, street furniture, and landscaping, grouping of specialized businesses or services, and promotional or cultural events held in the area. A theme helps add interest to a downtown and creates a unique point of destination. However, caution should be taken in ensuring that a themed downtown remains authentic and does not become a “theme park”. This can be addressed by ensuring that theme is grounded in both the present and historic use of the area, and enhanced with quality design and detail. A theme should not be overly prescriptive and does not have to apply to every element in order to be successful. A general sense of uniqueness can be made evident in the architecture of several, but not all, buildings and through streetscape enhancement to create a sense of identity and place.

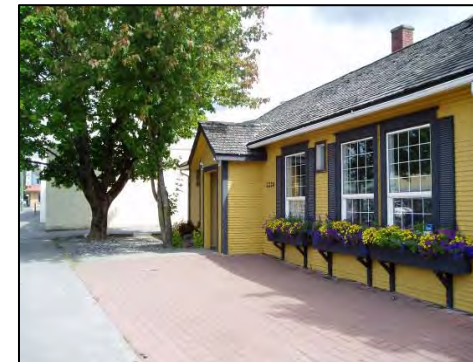
Terrace has adopted several bylaws and policies that together serve to establish a historic theme for the downtown area. This is consistent with the land use designation of

“Downtown Tourism” on Kalum Street between the George Little House and Lakelse Avenue. The OCP provides policies which support the historic theme as well as the concentration of pedestrian oriented retail businesses and cultural facilities. The George Little House, protected as a designated heritage resource, provides an anchor point at the south part of Kalum Street. The original B.C. Provincial Police building is situated on the Northeast side of Lakelse and Kalum. On the west side of Kalum Street between Greig Avenue and Lakelse Avenue, two historic commercial buildings exist: the original drug store and old Bank of Montreal building (which is now an angling shop). Unfortunately, three historic buildings on the west side of Kalum Street were destroyed by fire in the early 1990s. Redevelopment of those vacant lots should be consistent with the heritage theme of the area.

Both the George Little House and the B.C. Provincial Police building are recognized as municipal heritage resources. Several other commercial buildings in the downtown dating from the 1920s and 1930s are eligible for formal recognition by including these properties on the Community Heritage Registry. These buildings require renewal and improvement to showcase their historic legacy.



Kalum Ave in 1930s (upper) showing the drug store, and former Bank of Montreal. Below, the old BC Provincial Police building (middle) and George Little House (bottom).



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Examples of Theme

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Figure 4

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The 4600 block of Lakelse Avenue contains several buildings dating from the 1920s and 1930s. The south side of the 4600 block has maintained the majority of its historic lot and building sizes. These buildings and their use as street oriented retail and service, contribute to an attractive, interesting, and lively streetscape.

In the 1980s, as part of a “main street” improvement, an arcade was installed along the south side of the 4600 block of Lakelse. The arcade covers the sidewalk and features a sloped brown metal roof, with interspersed gabled extensions in front of each business.

The arcade provides a continuous, dominant, visual feature on the block which acts contrary to the ideal of an interesting, diverse and vibrant streetscape. Unfortunately, the arcade obscures the building facades on the street, including several buildings with potential heritage significance. This works contrary to the development of a historical theme for the downtown. The arcade provides protection from the weather, enhancing the pedestrian experience in rainy or snowy conditions. However, the arcade also blocks sunlight on the south side of the street, making it less enjoyable in Terrace’s fine spring and summer weather. The arcade is low in height (fails to meet current bylaw standards)

and creates a dark and confining atmosphere.

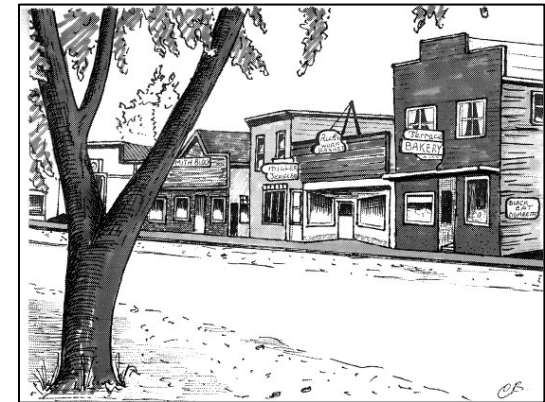
The “Main street” reconstruction conducted in the 1980s included pacer stone sidewalk, pedestrian scale lighting, street trees and miscellaneous street furniture. This block has not aged well. Specifically, the red brick paver stones were of a poor quality and have weathered significantly. The lighting has proven to be inadequate in design for both pedestrians and vehicle traffic on this block.

Directives

1. The City of Terrace shall strive to maintain and enhance the historic theme of the downtown.
2. New developments in the Downtown Tourism Area shall fit into the heritage theme and context.
3. The City of Terrace should consider eligibility for placing downtown properties on the community historic registry.
4. The City of Terrace shall develop a placard or signage system to identify heritage resources in the downtown.
5. The City of Terrace should investigate ways to encourage

and assist property owners in renewing and revealing the architectural elements of heritage buildings.

6. The City shall investigate options for assisting property owners in altering or replacing portions, or all, of the arcade over time.
7. New individual awnings or canopies that provide weather protection, increase day-lighting and reveal the existing building architecture shall be encouraged.



**Sketch of 4600 block Lakelse Avenue in 1950.
Credit: C. Bram.**

3.3 Beautification and Activity

Beautification elements go beyond streetscape and infrastructure improvements. Beautification elements demonstrate the vibrancy and spirit of a community. They provide flourishes of interest, which can be seasonally updated.

Beautification elements such as banners, hanging flower baskets, and public art may be added to existing or redeveloped streets and may enhance the entire downtown area. A banner program that creates unique banners to promote seasonal events can provide a fresh and changing appearance to a downtown throughout the year. Decorative lighting is another element that can add visual excitement to the downtown. One challenge to adding decorative lighting, banners, and hanging baskets is the use of BC Hydro poles for much of the city's lighting system. BC Hydro is reluctant to allow ornamentation or enhancements on these poles.

Activity in the public realm is a key element of a vibrant and attractive downtown. Programmed activities are an important way to attract the community and visitors to the downtown. Parades, farmer's markets and street fairs are excellent activities that bring vibrancy and life to the street. Residential

development within the downtown core also creates increased pedestrian traffic. An increased level of activity benefits local businesses. It can also have a positive effect on the perception of safety in an area.

Directions

1. Hanging baskets should be installed on light poles where possible to replace the existing street level concrete planters.
2. The City should contact BC Hydro to consider allowing the installation of enhancements on their poles.
3. A Banner and decorative lighting program should be initiated.
4. The City should endorse and support a variety of activities, festivals and events to be held in the downtown.



3.3.1 Business Improvement Area

A Business Improvement Area (BIA) is an area designated by bylaw in which businesses and property owners can finance effective marketing, promotional and revitalization programs. It acts as a self-help mechanism designed to assist local business people and property owners promoting their business and shopping district to further trade and commerce in their area. A BIA provides both the organized structure and the source of funds to enable local business communities to improve their commercial viability. Authority to create Business Improvement Areas is contained in the *Community Charter* under Section 215.

The establishment of a BIA occurs when a business community requests to be designated as an improvement area by its municipal council. Once established, the BIA can coordinate and directly enact specific activities and programs, such as promotion, cleaning, and security. Other examples of typical public improvements include tree planting, street furniture, improved parking, flower boxes and baskets, and street banners. In addition, BIAs

promote their areas as business and/or shopping areas, not just as a group of individual shops, restaurants and offices. Promotional activities may include special events and ongoing advertising programs which aim to keep the BIA area foremost in the public's mind as an attractive, pleasant place to shop, to be entertained, to work and to live.

Typical Business Improvement Areas activities include:

Marketing and Promotion – This involves promoting the area as a whole and could include: hosting festivals, concerts, and other special events; promoting the area to new businesses; providing maps, banners and guides; publishing newsletters

Maintenance – This could include trash clean up, cleaning up graffiti, snow shoveling, street sweeping, and landscape maintenance.

Security – This could include hiring a security service or installing surveillance equipment.

Policy Advocacy – BIA organizations can be an advocate for their interests to the local government.

Small-Scale Capital Improvements – Funding and implementing improvements to

the BIA are common and could include landscaping, street trees, and benches.

Appendix A contains more information regarding the establishment of a BIA.

Directives

1. The City shall consult with the Downtown Merchant Association and Chamber of Commerce regarding the establishment of Business Improvement Area in the downtown core.

3.4 Gateways

Gateways signify to motorists and pedestrians that they are approaching and entering a special or distinct area. They delineate a clear point of entry, create a sense of arrival and contribute to the positive image of a place. As such, gateways should be distinctive focal points, while at the same time tying into an established theme for an area.

The 1998 Downtown Assessment and Evaluation Study identified three major downtown gateways: Greig Avenue at Sparks Street; Eby Street at Highway 16; and Lakelse Avenue at Apsley.

Since the 1998 study changes have occurred in Terrace that has led to modifying the location of some of the gateways (see Figure 4). The Major Gateways are now identified as:

- Lakelse Avenue at Apsley Street (East).
- Eby Street and Highway 16 (West).
- Sande Overpass and Highway 16 (South).
- George Little Park at Kalum Street (North).
- George Little House on Kalum Street (VIA Rail Station).

The George Little House and its recently designed signage provide elements to incorporate into the other proposed “gateways” to provide continuity and consistency. Design treatments for each of the gateways will vary based on the existing and future context of each gateway. It is anticipated that common design elements will be incorporated into the design of each location. The design elements should be consistent with the historic downtown theme. Design treatments should feature repetitive, vertical streetscape elements that add color, and texture to the entry, such as signage, flags and banners, landscaping, and public art. Design elements should include signage, lighting, plantings, rock accenting, and if space allows, benches, waste receptacles and other street furniture (Figure 5).

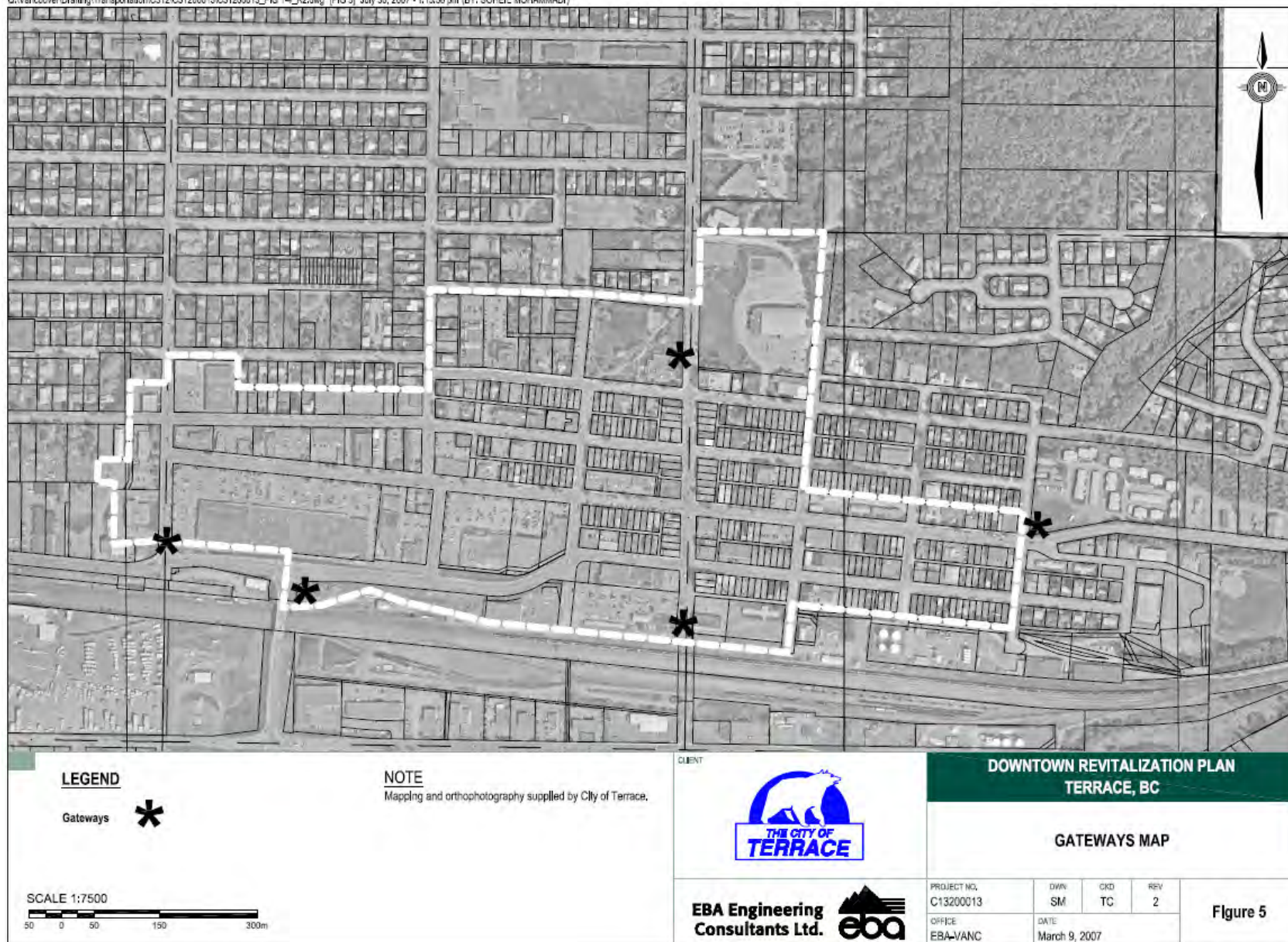
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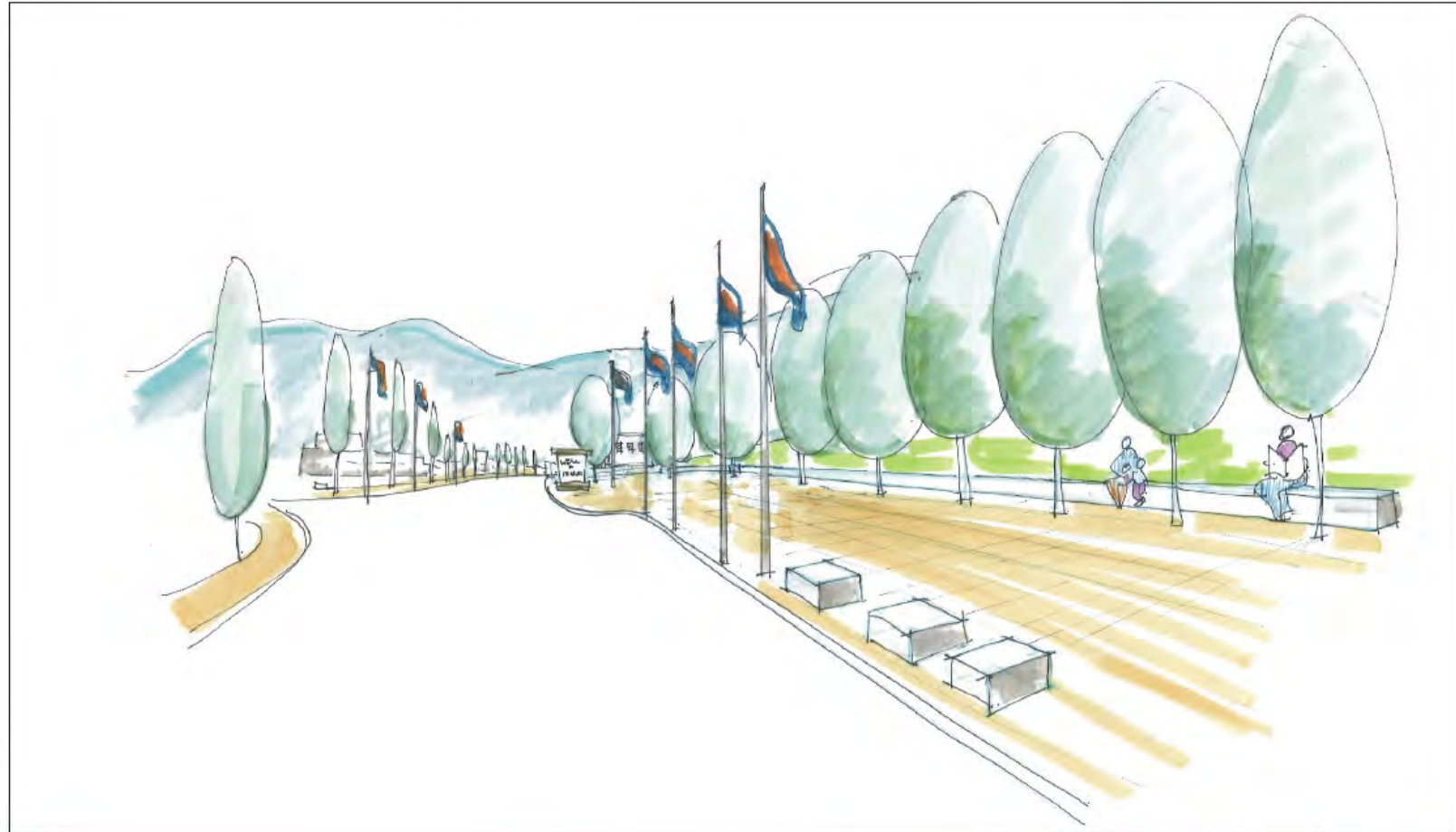
1. Gateways to Downtown Terrace shall be constructed which reinforce the downtown historic theme.
2. The City of Terrace should explore opportunities such as grants and service groups to provide funding for gateway development.



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**CITY OF TERRACE
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**Gateway Concept for Lakelse Avenue
at Apsley Street**

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Figure 6

3.5 Public Realm

The public realm is a crucial element to the success of a downtown. Healthy and vibrant downtowns provide a diversity of public spaces both in location and in type. In addition to public streets and sidewalks, Downtown Terrace has a variety of existing public spaces as well as opportunities to create others. Existing public spaces include:

- George Little House.
- City Hall.
- Upper and Lower Little Parks.
- Recreation complex (arena, pool, Skateboard Park).
- Farmer's Market.

Opportunities to enrich the public realm include the redevelopment of the old Co-op site, including a public plaza on the corner of Kalum Street and Greig Avenue, upgrades to Lower Little Park, as well as upgrades to the streetscape in general.

The City of Terrace is committed to upgrading and improving the public realm. Public realm improvements play an important role in downtown revitalization. Streetscape and public space improvements create a more attractive commercial district

and encourage pedestrian activity. It is also important to remember that investment in downtown streetscape improvements is an investment not just in the downtown but also in enhancing the perception of the entire community.

Public buildings are a highly visible element of the public realm. Government buildings, such as City Hall, public libraries, Provincial courts and offices should be structures that exhibit a high quality of architectural design. The location and visual character of public buildings can act as landmarks and provide orientation in a downtown district. Terrace's City Hall, located on Eby Street, could provide a dramatic landmark down Lakelse Avenue with an addition that incorporated an architectural feature, such as a tower (Figure 7).





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Concept for City Hall Improvement

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Figure 7

3.5.1 Public Spaces: Parks and Plazas

Parks and plazas are some of the more important public spaces in a city. Multi-use community parks provide many functions including sites for recreational and cultural activities, places for children to play, green space, linkages to destination points, a refuge from busy streets, and a social meeting and gathering place. Famous parks such as London's Hyde Park, New York's Central Park, and Vancouver's Stanley Park or famous plazas, such as Rockefeller Plaza in New York or Trafalgar Square in London, exemplify how great parks and plazas can define their cities and provide many amenities to residents and visitors. However, public spaces do not have to be large to be important. Parks and plazas can also be smaller, more intimate public spaces. Overall, public spaces play an important role in defining the identity of a city, and generate numerous benefits including:

- Improving the environment by providing open space, green spaces, and clean air.
- Promoting physical and mental health by providing outdoor recreation areas, playgrounds and refuges from busy streets.

- Economic value through increasing surrounding property values, attracting residents and tourists, and enhancing the general attractiveness of an area.
- Social value through gathering places and providing a venue for cultural events, such as concerts, festivals, and plays (Shakespeare in the Park).

Upper and Lower Little Parks are situated along Park Avenue and separated from one another by Kalum Street. Lower Little Park is a multi-use community park containing an amphitheatre, playground, field, greenery and landscaping. The library and art gallery are located at the corner of Park Avenue and Kalum Street, creating an excellent multiuse public park space.

Upper Little park slopes steeply up from Kalum Street. A recreation complex is located at the top of the hill, featuring an Aquatic Centre and Arena (soon to be twinned). The skateboard park in Upper Little Park is not visible from Lower Little Park or most of Upper Little Park. The location of the skateboard park is problematic because the lack of visibility poses a security issue.

A downtown urban plaza is designated in the OCP for the SW corner of Grieg Avenue and Kalum Street. The development of this public space on the former Co-op lands will add a new beneficial meeting space within the downtown.



Upper and Lower Little Parks were set aside as parkland in the original survey plan for Terrace by George Little. These parks comprise the oldest public spaces in the city. While the parks currently serve many purposes, improvements would enhance their use and general aesthetic.

Upgrades could include:

- improved landscaping and plantings, including a gateway feature on Kalum Street at the northeast corner of Lower Little Park.
- improvements to the playground equipment with new play features.
- additional benches and picnic tables.
- upgrades to walkways and adjacent sidewalks.
- Improved lighting.
- Addition of public art.

Redesign of part of Lower Little Park to incorporate the location of some activities, such as the skateboard park or a splash park, should also be reconsidered so as to enhance safety and inclusion.

Directives

1. The City shall seek opportunities for upgrading Upper and Lower Little Park, such as the BC Localmotion and BC Spirit Squares Grant Programs.
2. The City shall improve pedestrian connections between the Downtown Core and the George Little Park.
3. The City should improve linkages between Upper and Lower Little Parks and investigate the potential for a mid block pedestrian crossing on Kalum Street.
4. The City should investigate the potential for relocation of the Skateboard Park and development of other active and passive recreational elements as part of the park enhancements.



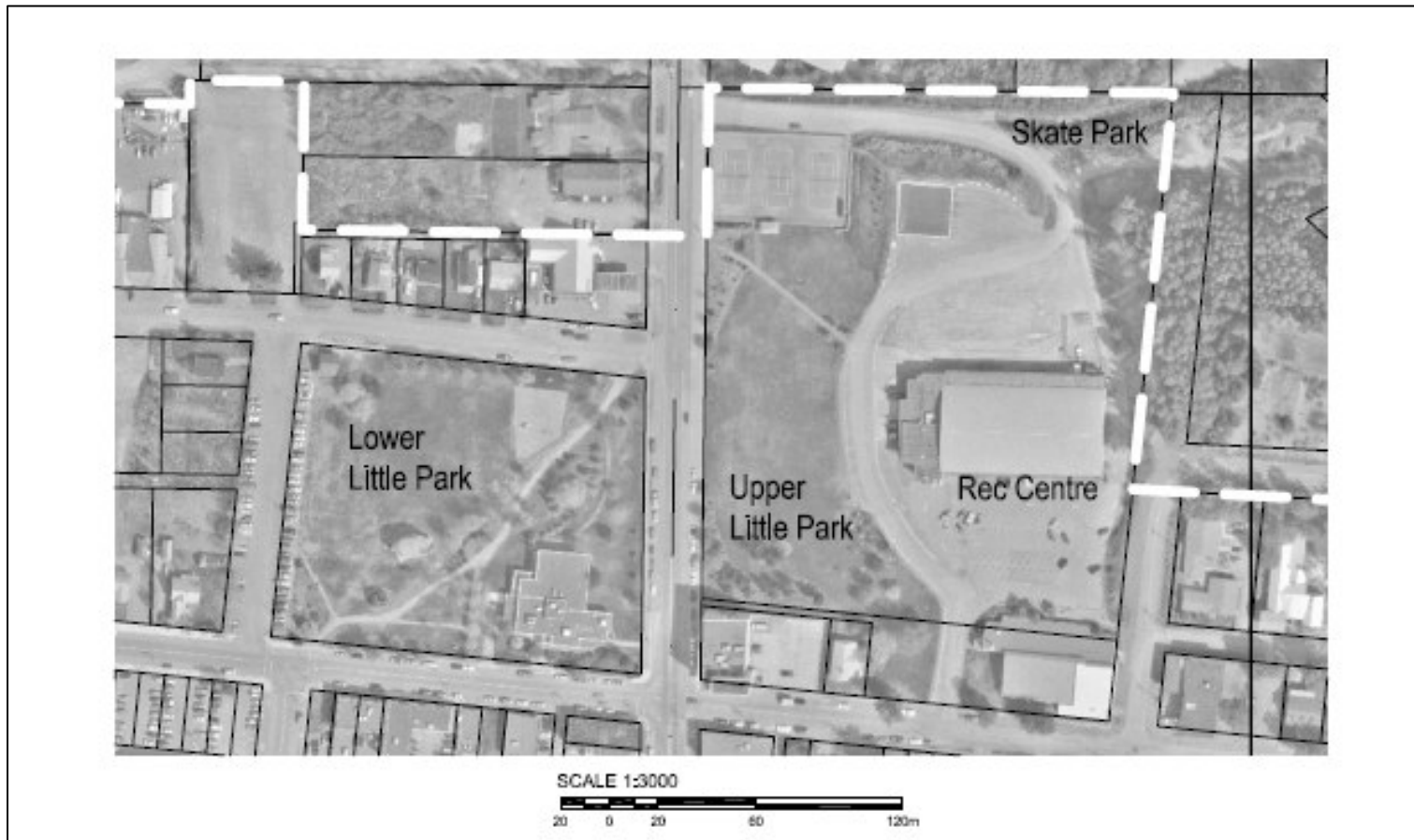


Figure 8 – Upper and Lower Little Parks

3.5.2 Streetscape Enhancements

A city's image is highly dependent on the quality and vitality of its public realm consisting largely of streets. Streets that form part of the public realm offer the best opportunity to visually connect new development with historic buildings and patterns of land use. Street reconstructions that have been completed since 1998 include a number of streetscape elements carefully selected to create a unified, attractive and pedestrian friendly downtown.

It is important that the City adopt a consistent approach to the design and implementation of streetscape improvements. A unified design treatment will improve the overall visual appearance of the downtown and contribute to more efficient maintenance of street furnishings through the use of standardized elements. To this end, the design, color, choice of material, and location of streetscape elements, have been selected to ensure a coordinated and attractive streetscape and to minimize vandalism and maintenance.

The following sections outline the different features of the downtown streetscape and provide direction for their enhancement. Figure 9 shows the proposed extent of the Downtown specific streetscape design. It is

anticipated that this streetscape renewal will be achieved over a 10 to 20 year time frame.

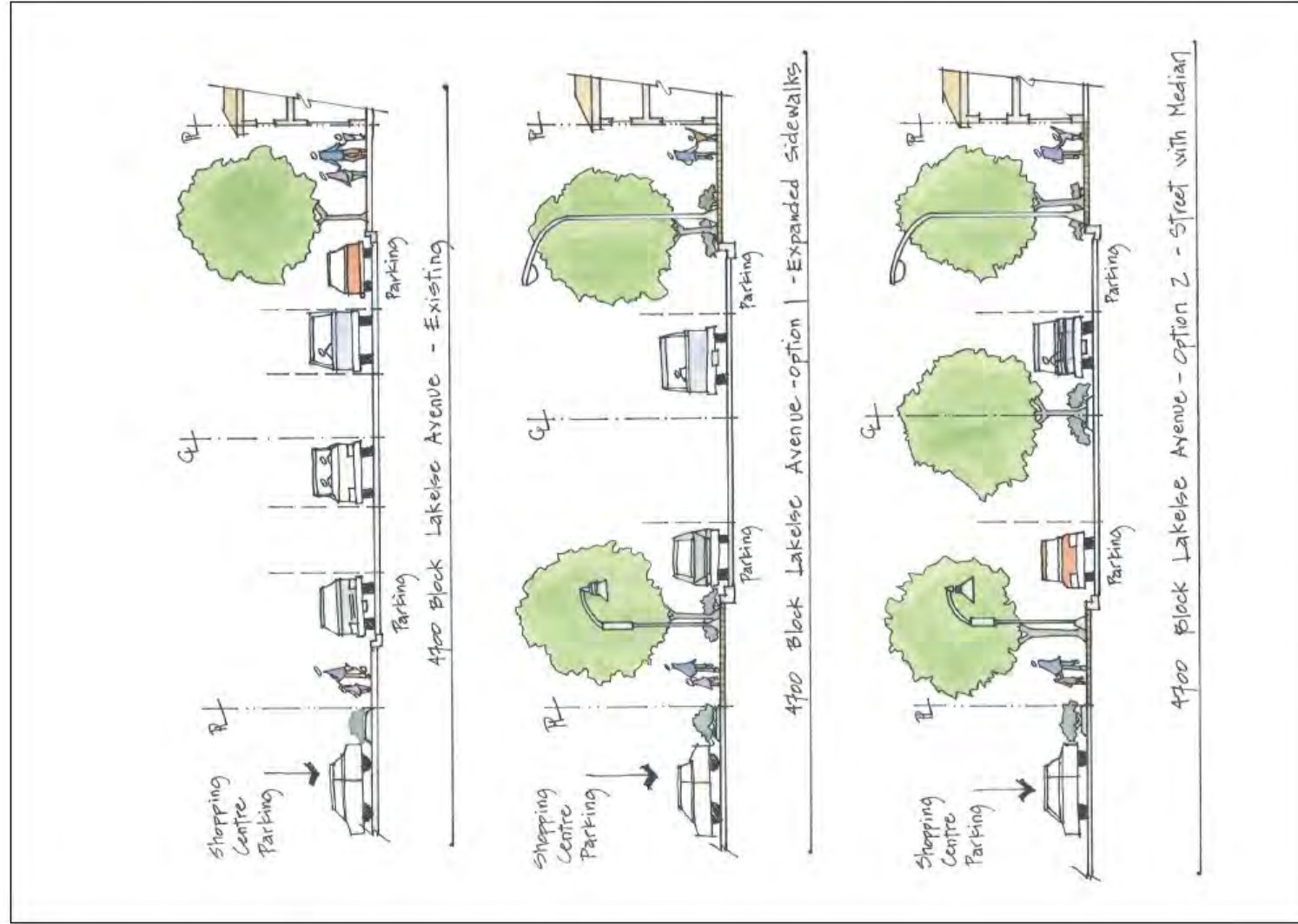
Figure 10 illustrates street redesign options for the 4700 block of Lakelse Avenue. The existing situation is contrast with Option 1, where sidewalks are expanded, and Option 2, where a median is added.

Directives

1. The City shall endorse a unified design of the downtown streetscape.
2. The City shall consider a downtown street for reconstruction every 1 to 2 years as part of its capital street improvements budget.
3. Priority of street reconstruction in the downtown will vary based on the condition of the existing infrastructure and upon major new developments proposed in the downtown.

DOWNTOWN PLAN





CITY OF TERRACE DOWNTOWN PLAN		Street Redesign Options for Lakelse Avenue	
DATE	DESIGNED BY	DRAWN BY	REVIEWED BY
	C31200013	MEZ	TUN
	OFFICE	DATE	0
	BBM-WMC	JUN 30, 2017	

EBA Engineering Consultants Ltd.		Figure 10	

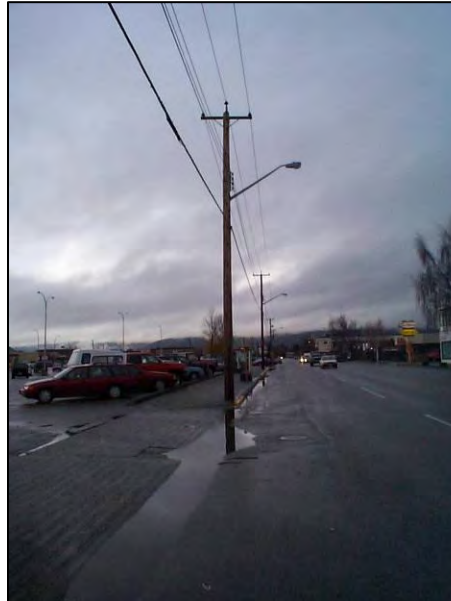
3.5.2.1 Street Lighting

Downtown Terrace has been updating its lighting system over recent years to improve both the design quality and illumination. The older light system featured two types of street lights: the decorative lighting system limited to the 4600 block of Lakelse Avenue and the overhead main lighting system found on all other streets within the study area.

The 4600 block decorative street lighting installed in the 1980s is marginally acceptable for pedestrian scale and ambiance, but does not adequately illuminate the roadway, intersections, and the mid block crosswalk. The overhead lighting found throughout the rest of the downtown core consists of cobra head light fixtures or davit arms attached to BC Hydro power poles. This form of lighting would be adequate for both road and sidewalk illumination if the average spacing of the lights were reduced and additional lights installed at intersections.

The existing cobra head lights deter from a pleasurable streetscape experience. They are not pedestrian in scale, are attached to wooden power poles, and not coordinated with street furniture. The existing lighting, attached to the power poles, restricts streetscape beautification enhancements as BC Hydro is reluctant to allow the placement

of banners, hanging baskets, or other ornamentation on their poles.



Older auto oriented lighting systems



Newer auto oriented lighting systems

In recent years, City street reconstruction in the downtown has incorporated improved lighting systems. The 4500 block of Grieg Avenue was rebuilt with streetscape improvements and incorporated a lighting system that is of sufficient strength and height, 7.5 m (25 feet) to allow for banners, hanging planters, or a combination of both. The 3200 block of Kalum Street was improved with lighting that is oriented for pedestrians and suits the heritage theme for the Downtown. This street utilized two types of pole lighting to provide adequate street illumination for safe vehicle movement and to enhance the street character and pedestrian experience.

Directives

1. New lighting should be installed on all streets within the study area at the time of street reconstruction.
2. New lighting shall be functional for both automobile scale and the pedestrian scale.
3. New lighting should reflect the historic theme and character of the downtown.
4. New poles shall incorporate power outlets to facilitate ornamental lighting displays.



Older pedestrian oriented lighting systems



Newer pedestrian oriented lighting systems

DOWNTOWN PLAN

3.5.2.2 Pedestrian Areas

Sidewalk Treatment

Sidewalks are the primary pedestrian area of the public realm. Wide sidewalks, with adequate space for pedestrian movement, street furniture and landscaping elements enhance the sense of comfort and safety of the pedestrian experience.

The purpose of the sidewalk design treatment is to unify the downtown streetscape. By applying the same design treatment throughout the downtown, the area is defined and linked together in an appealing pedestrian friendly manner.

Three existing sidewalk surface design treatments have been implemented in the downtown. Standard concrete sidewalks encompass the vast majority of the downtown area except in the 4600 block of Lakelse Avenue where red unit pavers were installed in the early 1980s as part of a main street redevelopment program, and around the Kalum Street/ Greig Avenue intersection, along the west side of Kalum Street between Park Avenue and Lazelle Avenue and the 4500 block of Greig Avenue where autumn and charcoal color unit pavers were installed during projects in 1999, 2001 and 2004.

The red unit pavers installed on the 4600 block of Lakelse Avenue were discontinued

due to the poor quality of paver. Inspection of the bricks shows significant wear on the pavers and replacement will need to be carried out sooner than expected.

The autumn and charcoal pavers were chosen because of their aesthetic versatility in both the older and newer sections of downtown. The installation pattern with a 2 brick charcoal banding “breaking up” the herringbone autumn base provides interest and detail in the sidewalk surface. It was also felt that the use of a more natural color paver in an attractive two colour pattern would better compliment the diversity of buildings throughout the entire downtown. These unit pavers installed on the recent sidewalk reconstruction projects are expected to have a long life span thereby reducing future costs

Directives

1. The streetscape reconstruction in the downtown should incorporate widening of sidewalks wherever feasible; in some situations this may include reducing the number of vehicle traffic lanes.

2. The City of Terrace should install the autumn and charcoal pavers as the sidewalk design treatment on all pedestrian areas throughout the ***Downtown Plan*** area at the same time as future road reconstruction.



Sidewalk Bulbs and Pedestrian Crossings

Wide sidewalks enhance the comfort for pedestrian movement in a busy urban setting. Sidewalk bulbs and widened sidewalks can assist in traffic calming. Sidewalk space should be increased at all intersections to allow for safe pedestrian movements both on the sidewalks and while crossing streets. The sidewalk bulbs clearly define intersections, shorten pedestrian crossings, create space for pedestrians waiting to cross streets without conflicting with those traveling on the sidewalk, provide space for street furniture, public art and landscaping elements, and slow down vehicle traffic.

Sidewalk bulbs currently exist on the 4600 block of Lakelse Avenue, and on Kalum Street and Greig Avenue. The new style of unit paver should be used in the surface treatment of all sidewalk bulbs to unify and compliment the proposed sidewalk design treatment.

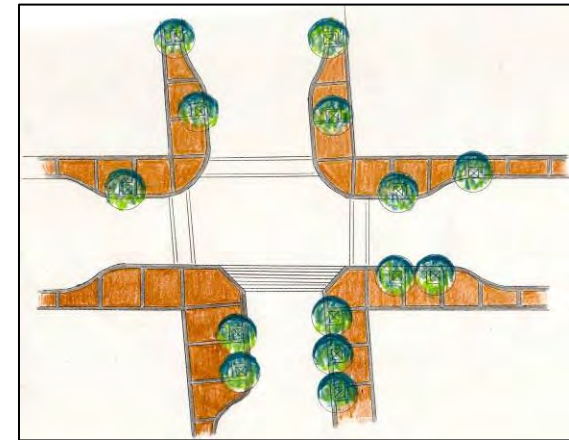
Sidewalk bulb design will generally follow the design standard of the recently installed bulbs at the Kalum Street/Greig Avenue intersection (see Figure 5). Adaptations to standard length, width, and radius may be required at high traffic intersections such as Lakelse Avenue and Kalum Street and at skewed intersections such as Greig Avenue

and Emerson Street and Lakelse Avenue and Sparks Street.

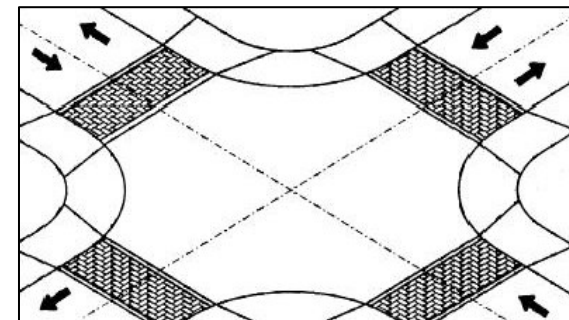
Curb cuts should be provided in the sidewalk bulbs to improve accessibility for residents who are less mobile, people who use wheelchairs, and parents with strollers. Curb cuts should be located where the crosswalks connect to the sidewalk bulbs.

The purpose of a pedestrian crossing is to clearly delineate pedestrian space, to visually and functionally extend the pedestrian environment into the street and to encourage drivers to reduce their speed at mid-block and intersection locations. Consideration should be given to utilize concrete and unit pavers in the on-street crosswalk design instead of the typical painted marking. This could further highlight the intersection and enhance pedestrian safety.

Proposed sidewalk bulb locations as illustrated on the detailed downtown streetscape demonstration plan (Figure 6) have been determined following a preliminary assessment of traffic flow, transportation requirements, turning lanes, and major pedestrian routes.



Example of sidewalk bulbs



Example of paver stone pattern

Currently the City has two mid-block pedestrian crossings within the study area. These are located on the 4600 block of Lakelse Avenue and at the McDonald's Restaurant and the Skeena Mall in the 4700 block of Lakelse Avenue. Both of these crossings require upgrading to make them safer for pedestrian use.

The mid-block crossing in the 4600 block of Lakelse Avenue requires increased lighting and a bulb out constructed along the north side of Lakelse Avenue to allow motorists to see pedestrians crossing from the north and to shorten the crossing distance.

The "McDonald's/Skeena Mall" mid-block pedestrian crossing is well lit, but the width of the crossing is extreme. This forces pedestrians to cross several lanes of vehicular traffic, allows traffic to travel at high speeds, often above the posted limit, and forces pedestrians to wait on the sidewalk behind the vehicles parked at the curb and out of drivers' views. Bulb outs should be installed on both the north and south sides of Lakelse Avenue at this location to reduce the travel distance for pedestrians and to allow both pedestrians and vehicle to have better sight lines of one another.

The bulb outs should be of sufficient length to allow vehicular traffic to see pedestrians at a safe distance from the crossing.

Sidewalk bulbs should also add to the enjoyment of the streetscape aesthetically by incorporating the sidewalk paver stone treatment, street furniture, and landscaping.

Directives

1. The City of Terrace should install sidewalk bulbs at all downtown intersections at the time of future road reconstruction.
2. Streetscape reconstruction in the downtown should incorporate widening of sidewalks where feasible.
3. Mid block pedestrian crossings should be incorporated in the design of streets that have an overly long block length.



**Mid block crosswalk on the 4600 block
Lakelse**

3.5.2.3 Street Furniture

In conjunction with recent upgrades on the Kalum Street/Greig Avenue intersection and the 4500 block of Greig Avenue, a combination of street furnishings has been selected for the downtown. Street furnishings include a wide variety of items that can be used to both enhance and improve the function and aesthetics of the downtown. This section focuses on the most commonly used items, such as, benches, waste receptacles, planters, street trees, street tree grates and guards, and bike racks. In addition to other street furniture elements, Public Art can provide visual interest and opportunity to showcase local artists' works and talents.

Benches

Benches are provided in downtowns to serve as resting points and, if sited appropriately, to encourage social interaction. Currently there are few benches in the downtown with the exception of the 4600 block of Lakelse Avenue and at City Hall. It is proposed that new benches be installed throughout the downtown.

The selected bench follows the design theme for the downtown, is similar to that found on the Grand Trunk Pathway and is proposed to be the bench of choice in Lower Little Park.

Bench placement must include consideration of proven urban design principles. It is proposed that new benches be located in selected sidewalk bulbs, in bump outs, at gateways, and in public/private nodes. Placement locations should be coordinated with Public Works to ensure snow removal from downtown sidewalks is not impeded. Benches should also be near waste receptacles to help minimize litter. Where space permits benches should be placed in pairs to encourage social interaction.

Members of the community and business owners are encouraged to consider the Leisure Services Department Park Bench Donation Program. Benches donated through this program may be placed in the public parks or throughout the downtown area at pre-approved locations.

Directives

1. Benches and/or concrete bench pads should be installed at various downtown locations at the same time as future road construction or during sidewalk reconstruction.
2. Additional benches should also be installed when funds become available through the park bench donation program.



Example of Bench

Waste Receptacles

Attractive well-designed and appropriately located waste receptacles are essential to control litter and reduce street maintenance. Waste receptacles should be coordinated with other street furnishings. Waste receptacles should be located close to high use areas, seating areas, bus stops, and restaurants, but not placed so as to interfere with pedestrian movement and snow clearing functions.

Existing pole mounted wire mesh waste receptacles, located throughout the downtown should be removed and their existing locations reassessed when new receptacles are sited. Other existing concrete base waste receptacles should be relocated outside the downtown and replaced with the new waste receptacle of choice.

Generally there should be a minimum of four waste receptacles per city block, placed on each side of the street, to be located in conjunction with benches, in public nodes and near high-use pedestrian areas, especially where food is sold or in front of coffee shops.

The photograph on this page illustrates the type of waste receptacle selected for the downtown. The design and color

compliment the unit paver design and the other street furnishings.

Directives

1. Waste receptacles should be installed at various downtown locations in accordance with bench placement, concurrent with removal of an older type of existing receptacle, or during future road or sidewalk reconstruction.



Example of Waste Receptacle

Planters

Planters contribute color, texture, and add life to downtown streets. They are a major element of any streetscape design. Currently there are two different styles of planters used in the downtown.

Approximately 20 years ago the Greater Terrace Beautification Society contributed many round concrete portable planters throughout the downtown. The biggest concentration are on Eby Street between City Hall and Highway 16 and on the 4600 block of Lakelse Avenue. In addition to the Beautification Society's planters, the City has recently installed in-sidewalk ground planters (planting beds) on the east end of the 4500 block of Greig Avenue and hanging baskets on the 4600 Block of Lakelse Avenue and 3100 Block of Kalum Street.

Each of the types of planters can continue to be utilized to complement the downtown street furnishings and continue to provide color and texture to the area. This being said the location of the planters should be carefully considered and coordinated in order to build a sense of cohesive design while allowing snow removal to happen in an orderly timely manner. Over time the older concrete surface planters will be replaced with permanent landscaping beds or hanging baskets.



Older Concrete Planters



These concrete planters should be relocated as necessary, and located in groups throughout certain blocks to ensure cohesiveness. The planters on the 4600 block of Lakelse should be removed or relocated adjacent to the street trees in order to solve the winter snow removal problems. These old planters will be phased out as the City's hanging basket program is developed and as street reconstruction occurs.

The in-sidewalk planting beds should be installed in the sidewalk bulbs throughout the downtown. These planters, in association with bright flowering hanging baskets, add interest to the streetscape, increase the greenery, and compliment the street tree planting. An enhanced and well maintained urban forest includes landscape beds to soften the hard urban surfaces of concrete and asphalt.

Directives

1. Sidewalk planting beds should be installed within sidewalk bulbs at intersections in the downtown, at the time of future road reconstruction.
2. The Beautification Society's older concrete planters should be relocated to alternative locations within the downtown or

to Ferry Island as street reconstruction occurs, or when hanging baskets can be provided.

3. Hanging baskets should be installed on all reconstructed streets to provide seasonal color and complement the street trees and landscape beds.



Hanging Baskets



In-sidewalk landscape planting bed

Street Trees, Grates, and Guards

Trees are a major streetscape design element. They provide aesthetic value, protection from the sun, wind and rain, and improve air quality. When planted in rows, they help delineate space and create a unified streetscape. Street trees at curb side act as a buffer between vehicles and pedestrians. Trees also serve to soften the architectural edge of buildings and the hard surface urban environment through plant form, texture, foliage and color. Street trees are a key component of the urban forest and add unmatched beauty to the urban landscape.

In Downtown Terrace Street, trees are currently placed within the boulevard or sidewalks, in open spaces and parks, and as part of the edge treatment for newer downtown parking lots. The existing plant material is generally in good health.

Depending on the site conditions, individual planted trees should be spaced approximately 20 m apart. Within intersection sidewalk bulbs trees may be planted as close as 10 m apart. To make intersections uniform the first tree in a block should be planted 5 m back from the corner property line.

Trees selected for plantings shall be of a deciduous variety and should have a

minimum caliper of 4 to 7 cm. This caliper provides a tree with an acceptable canopy and gives the tree protection against vandalism. This size is specified to allow the tree to fit comfortably within the tree well and allow the root ball to support the canopy without undue stress.

It is recommended that the tree guards and the 1.2 m² pre cast metal tree grates used on the 4500 block of Greig Avenue continue to be used throughout the downtown. It should be noted that both the grates and guards must be treated with a black two-part epoxy coating. The coating prevents rusting and keeps the guards and grates looking aesthetically pleasing. To encourage downward root growth each tree pit should be planted within a manufactured tree well (concrete or vinyl). The tree wells are vital to prevent surface roots from bulging pavement, impeding shallow utilities and damaging sidewalks, and to reduce settlement around the trees.



Tree species should be selected taking into account tree canopy shape, pollution resistance, drought resistance, climatic considerations, leaf size and shape, maintenance, etc. Species of trees should vary from street to street to set a theme for each street and to reduce the impact of potential diseases. Some suggested varieties include Birch, Little Leaf Lindens and Honey Locust among others.

Sidewalk planting beds should be planted with a combination of low maintenance coniferous trees, coniferous shrubs, and deciduous shrubs. The minimum height of coniferous trees, where planted, should be 1.2 m. Large feature rocks may be incorporated into the beds to provide variety and interest. Decorative lighting can be introduced in the winter months to compensate for lack of leaves and to provide illumination in the additional hours of darkness.

It is also understood that new tree planting may be provided with the redesign of parking lot edge treatments, gateway development or town square development. In addition, street tree plantings should be encouraged along the site perimeter or in the road allowance during redevelopment of adjacent private property through the development permit process.

Finally, the existing street tree maintenance program should be reviewed and, possibly, increased to assure the proper care of street trees including adequate pruning and weed clearing around existing tree grates.

Directives

1. The City should install street trees, grates, and guards during future road reconstruction, at the time of adjacent commercial development or as a specific capital project on some downtown streets.
2. The City should ensure adequate resources are allocated, on an annual basis, to maintain the urban forest and ensure this valuable and beautiful element remains healthy and thrives.



Bike Racks

Bicycles provide an important alternative to vehicle transportation, as well as an efficient and healthy means of commuting. Bicycle travel needs to be supported by the provision of appropriate infrastructure, such as on-street cycling lanes and bike racks. The purpose of bike racks is to provide a facility where bicycles can be parked and securely locked, in a minimum amount of space. Racks should be accessible, durable, functional and attractive.

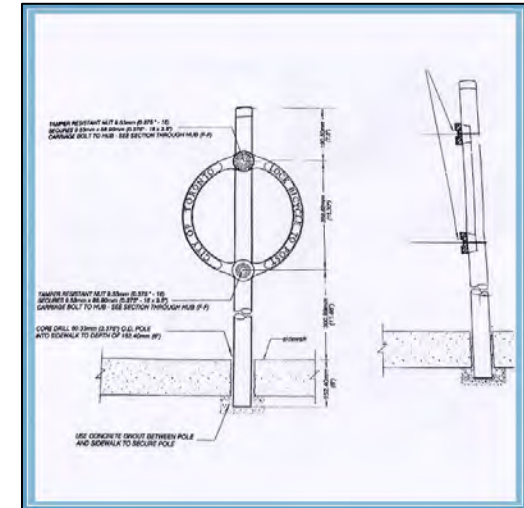
It should be noted that the Official Community Plan requires the provision of bicycle racks as part of any new development or redevelopment proposal. Currently there are few bicycles racks located within the downtown core. Cyclists resort to utilizing other items such as sign posts to secure parked bicycles often impeding pedestrian movement in the process. New racks should be located within sidewalk bulbs, at major destination points and public nodes. Bike racks should also be placed in downtown parks or plazas, such as the George Little Park.

City Council has recently endorsed the installation of “post & ring” style bike racks throughout the downtown and at municipal buildings. These attractive galvanized aluminum racks can securely accommodate

two bikes while allowing free pedestrian movement and unimpeded snow removal. The cast aluminum ring includes the marker “City of Terrace”, as well as directions for use as bike parking. The aluminum construction will ensure a long life and low maintenance while retaining an attractive appearance.

Directives

1. Bike racks should be installed at time of future road reconstruction or, as funds permit, at approved locations throughout the downtown.
2. A bike Rack sponsorship program should be considered to allow local businesses to pay for the cost of having a rack installed in front of their premises if so desired.



Example of Bike Racks



Public Art

Public art provides an attractive and creative display, which also offers a high quality enhancement to the public realm. Types of public art can include:

- Sculptures.
- Monuments.
- Murals or paintings.
- Mosaics.
- Fountains.
- Earthworks.
- Mixed media.

Public art can include both temporary and permanent installations. Public art, such as Spirit Bears in the City, can add a Visual, playful and creative aesthetic to the public realm. Public art pieces can be incorporated both in the public and private realms.

Directives

1. The City should explore the potential to develop a municipally funded and managed Public Art Program.



Spirit Bear



Mural at 3100 Kalum Street

3.6 Building and Property Improvements

Downtown Terrace is the heart of the community, with strong ties to the City's history. The *Downtown Plan* promotes a comprehensive approach to enhancing and revitalizing the downtown area. Physical enhancements to both the public and private realms, in addition to economic and social considerations will enhance and strengthen the downtown.

Maintenance and improvements to both public and private buildings and properties is paramount. This corresponds to the vision of a downtown that remains attractive to both tourists and residents.

Downtown Terrace contains several properties and buildings that are currently vacant. They range in scale and prominence, from smaller vacant storefronts, vacant gas stations and corner lots, to large vacant buildings and properties in prominent locations, such as the former Co-op department store site. The condition of some vacant properties requires improvement as properties that are not maintained detract from the attractiveness and vibrancy of the downtown. These properties can also act as magnets for property damage and vandalism, and can

impact the overall perception of public safety in the downtown.

3.6.1 Old Co-op Site

The old Co-op site is a large property located between Emerson Street and Kalum Street, and between Grieg Avenue and the Railroad tracks. In its hey day, the Co-op department store was the main anchor to the downtown commercial area.

The old Co-op was closed in 2000, leaving the building and site vacant. The City purchased the property in 2005, with the intent to have more direct control and to facilitate future development on this key site. The community has had a very active interest in the site, coming forward with several proposals for possible uses, including:

- Hotel, serving upper end clientele.
- Conference centre.
- Mixed use residential and retail.
- Municipal Parking facility – RV/Bus day use only.
- Auditorium/Theatre.
- Café/restaurants/pubs.
- Public Plaza.
- Artist workshops and 1st Nations carving centre.
- Cultural Facility – Gallery/Museum.

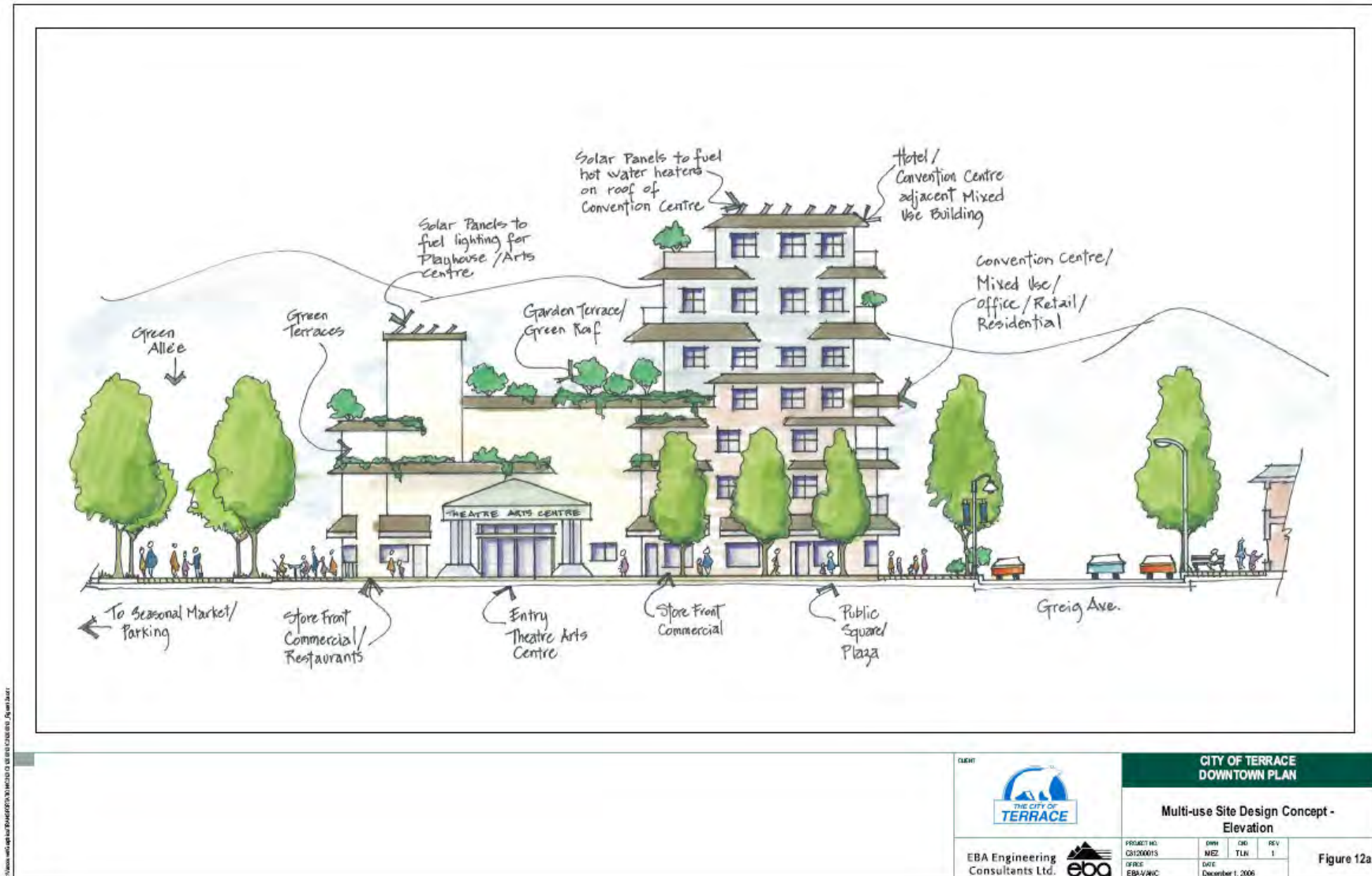
- Day-care Facility
- Public market/Farmer's Market.
- Community/Youth centre.

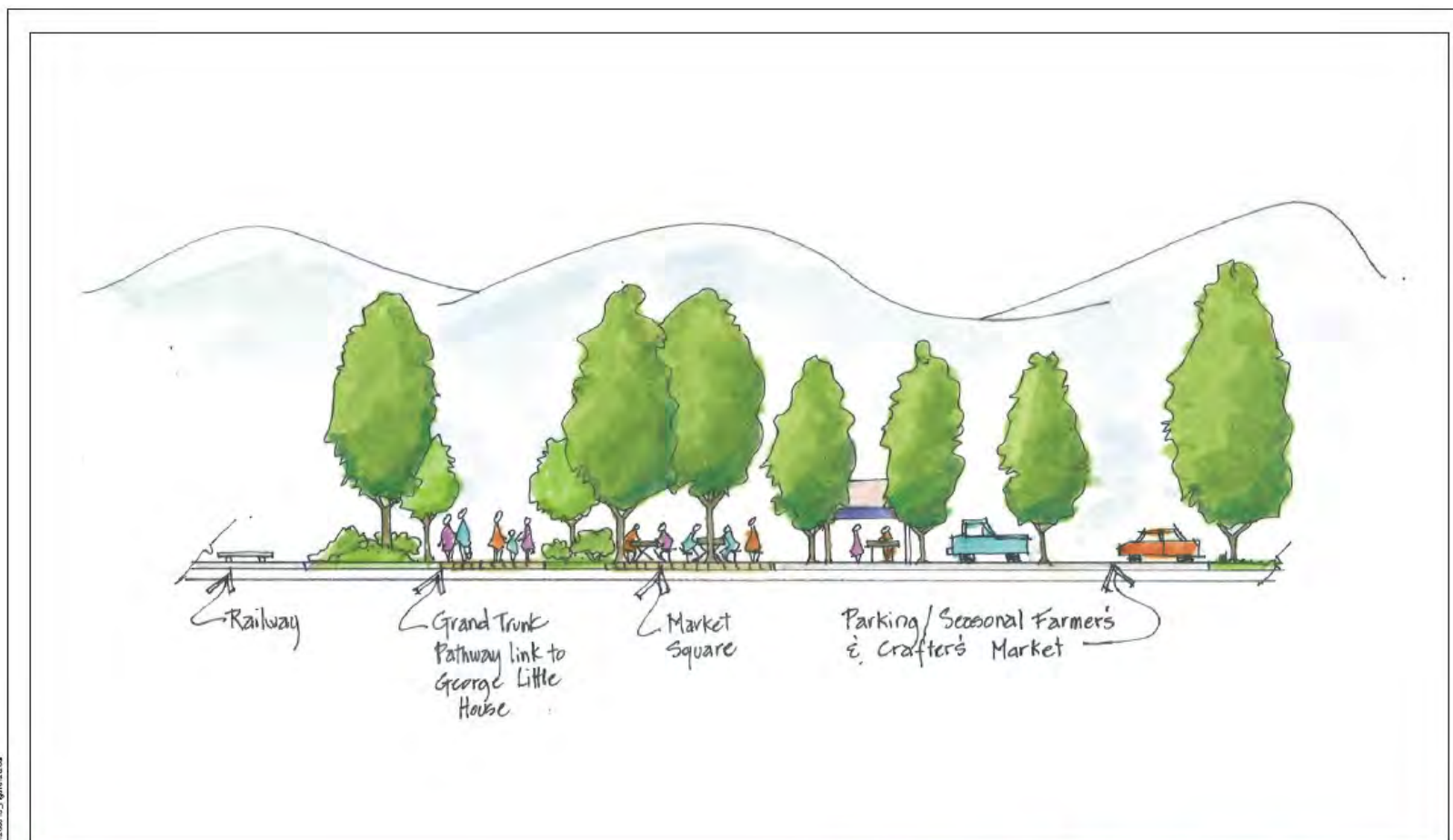
The site is of importance to the community due to its central location and almost limitless potential. It is important that the planning and development process for the site consider community input.

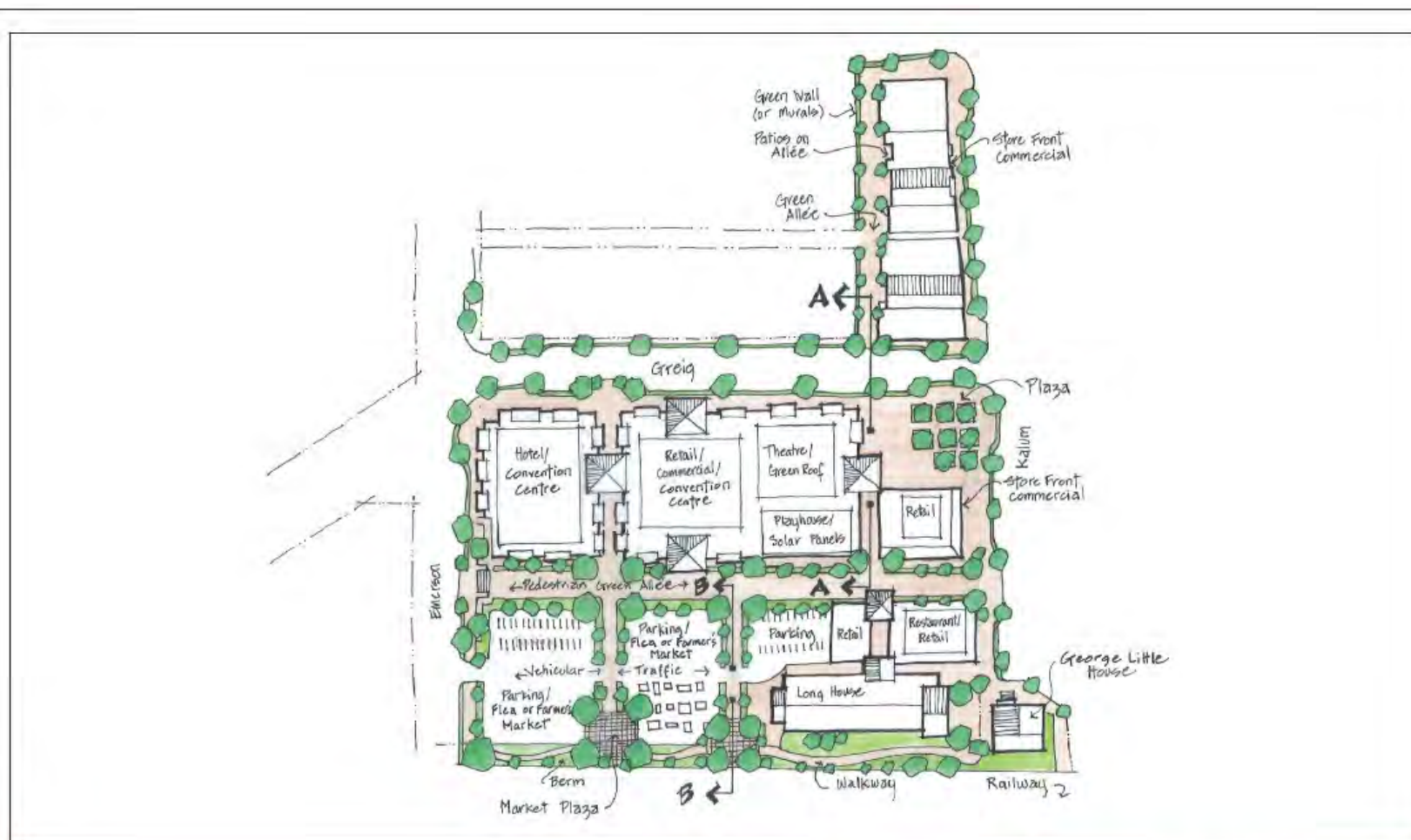
A mixed use development which incorporates such uses such as a hotel and conference centre, street level retail, cafés, an artist gallery and studio space, office space, multifamily residential, and a theatre would be appropriate for the site.

Directives

1. The planning and development process for the old Co-op site shall consider community input.
2. Development should incorporate a mix of uses to best achieve the Vision, including either a hotel and conference centre or a residential component.
3. The City should commission a comprehensive analysis to evaluate the potential costs and benefits to develop a Conference Centre on this site







3.6.2 Safety, Crime and Vandalism

In general, Downtown Terrace is a relatively safe and inviting area. However, like many other downtowns, Terrace does face issues of crime and vandalism. Section 1.3.3 of the OCP discusses safety and crime prevention and highlights city initiatives to improve the street lighting downtown, as well as adopting traffic calming measures.

Vandalism in the downtown area has been an ongoing concern in recent years. Vandalism in the downtown is not confined to one or two areas but is widespread. The number of vandalism and property damage offences in Terrace between 1996 and 2005 has been consistent at approximately 400 reported occurrences per year. This is significantly less than other similar sized northern communities, including Dawson Creek, Prince Rupert, and Williams Lake.

Dealing with vandalism and public mischief issues often requires multiple approaches, including education, social programs, actions of the criminal justice system, and opportunity reduction. While all approaches are important, the first three are primarily outside the scope of this plan. Opportunity reduction is a process to reduce crime and vandalism by removing the physical elements that promote or provide ease in

committing vandalism or a criminal act. There are a range of methods for reducing vandalism opportunities including formal surveillance, video cameras, increased pedestrian traffic (eyes on the street), and urban design elements that follow Crime Prevention Through Environmental Design (CPTED) Principles. Successful approaches have been proven to utilize a combination of these methods.

A previous vandalism report completed in 2006 by city staff confirmed that many of the broken windows occurred due to large rocks found adjacent to the property. This highlights the need to consider opportunity reduction methods in landscaping of both developed and vacant properties.

CPTED refers to a group of strategies intended to reduce both the perceived fear of crime and the opportunity to commit crimes, such as break and entry, vandalism, assault and vehicle theft.

The six key principles are:

Defensible Space – identifying ownership of space through real and symbolic barriers, and increased opportunities for surveillance.

Natural Access Control – deters access to a potential target and creates a sense of risk to the offender.

Natural Surveillance – placement of physical features, activities, and people to maximize natural visibility and observation. This is related to the “eyes on the street” concept.

Territorial Reinforcement - Defines clear borders of controlled space from public to semi-private to private, so that users of an area develop a sense of proprietorship over it.

Management and Maintenance – ensure that buildings and spaces maintain a standard of care consistent with local standards.

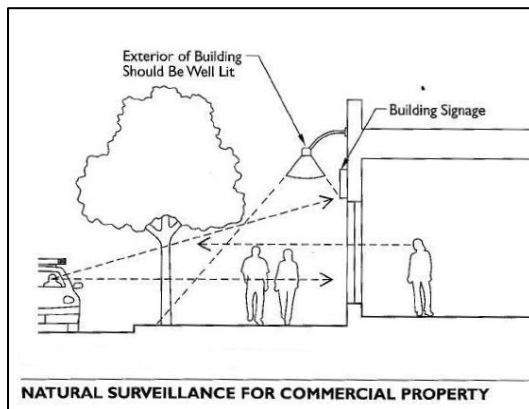
Legitimate Activity Support – encourages the intended use of the public space and helps discourage illegal activities.

The guidelines encourage generating more street traffic, to increase the number of “eyes on the street”. To accomplish this, the guidelines encourage more mixed use developments and enhanced streetscapes to encourage pedestrian traffic. In particular, increase residential properties in the downtown to ensure more activity and surveillance throughout the 24 hour day. Natural surveillance can also be increased by open sight lines and improved lighting. The guidelines also encourage creating a sense of ownership over public spaces. This

can be done in several ways, such as increasing pedestrian traffic, and extending the sense of ownership from residential areas and commercial areas into the public realm.

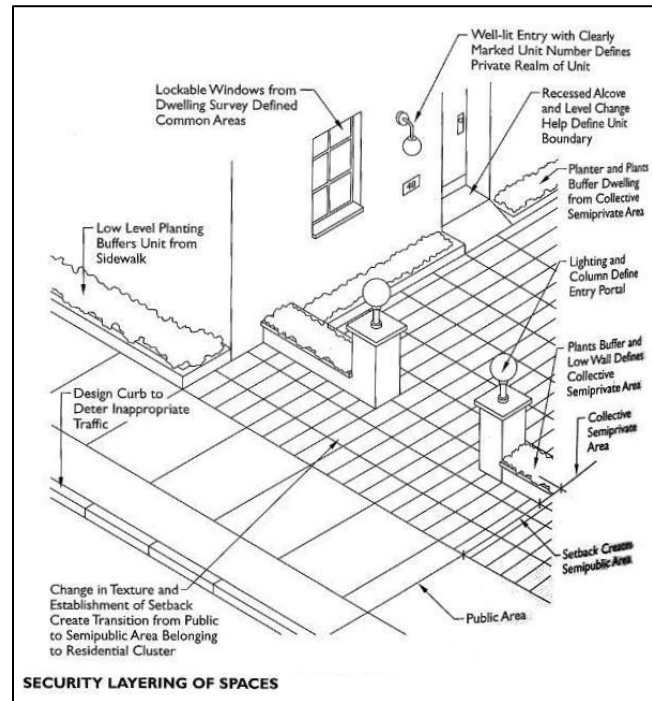
Directions

1. CPTED guidelines found in Appendix A should be considered for any redevelopments or enhancements done downtown.
2. Property owners should practice opportunity reduction by not using large rocks, stones, or moveable bricks greater than 2 cm diameter for landscaping and by ensuring vacant properties are maintained in a neat and orderly manner.



Natural Surveillance Example

Source. APA. 2006. *Urban Planning and Design Standards*. Hoboken, NJ: John Wiley and Sons Inc.



Example of Design Featuring CPTED Principles

Source. APA. 2006. *Urban Planning and Design Standards*. Hoboken, NJ: John Wiley and Sons Inc.

3.6.3 Revitalization Area

A Revitalization Area is an area where revitalization tax exemptions can occur. Revitalization tax exemptions are a tool that municipalities can use to encourage certain types of property investment and achieve development objectives, including building façade improvements and beautification. Authority to provide revitalization tax exemptions is contained in Section 226 of the *Community Charter*.

A downtown revitalization tax exemption area should be investigated as a means of promoting private property improvement and to foster new commercial and residential development in the downtown. The tax incentive on development would ensure the downtown core becomes an attractive option for investment.

Refer to Appendix A for specific information and examples of Tax Revitalization Areas.

This should apply to both new developments and substantial improvements to existing property.

Directives

1. The City shall investigate the use of revitalization tax exemptions to promote private property improvement in the downtown core.

3.7 Downtown Residential and Mixed-Use Development.

A vibrant downtown should offer the opportunity for residents to live, work, and play. Downtown residential development is key to the successful revitalization of a downtown and assist in a sustainable and robust downtown economy.

Residential development, grounded in market building strategies, is one of the most powerful tools available to strengthen the downtown. Residential development in the downtown increases the daily cycle of activity from the normal “9 to 5” business hours to a “24 hour” urban neighbourhood. This is not only a more efficient use of city infrastructure; it expands the market for downtown businesses and merchants. Downtown residents create a more diverse market by demanding a variety of products and services including retail, restaurant and entertainment facilities. Restaurants and cafes have an incentive to stay open later, which in turn encourages more “life on the street”, particularly in the evening hours.

Downtown residents are also more likely to take part in community activities downtown and take an active role on downtown improvement issues. The increase of activity, as well as a sense of neighbourhood

ownership, assist with crime prevention and opportunity reduction, and are consistent with CPTED principles. The opportunity to live and work downtown will also reduce reliance on vehicle transportation.

Downtown Terrace should offer opportunities for housing that appeal to a variety of groups including single people, young families, seniors, and students. Providing housing for both the rental market and home ownership is also important to ensure a range of housing options for residents of all incomes.

The OCP provides for the following downtown residential areas:

Village Residential – medium density housing including townhouses and walk-up apartments.

Multi-Family Residential – higher density housing including apartment/condominium buildings and townhouses.

Live/Work Studios – permit local business operators and craftspeople to work from their home in expanded home based businesses, home occupations, or home industries. This applies to the east portion of the downtown along the south side of Greig Avenue between Clinton and Apsley Streets.

Due to the compact nature of Terrace’s downtown and close proximity of different land uses, much of the residential

development in the downtown could also be part of “mixed use” developments.

Mixed use developments create vibrant urban environments that bring together compatible land uses and amenities, creating more pedestrian oriented, environmentally friendly destination points.

The compact street pattern and smaller lot sizes of the ***Downtown Plan*** area are well suited for mixed-use developments. Due to limited land supply, vertical mixed use is suggested. The lower levels of a building, including street level are more publicly oriented and should have commercial uses, such as retail or office space. Private uses, such as apartments, will be on the upper levels. To increase privacy, upper levels can be stepped back from the street.

Directives

1. The City shall investigate strategies to stimulate residential development downtown, such as reduced development fees and a revitalization tax area.
2. Density Bonuses shall be considered for multi-family residential developments in the core downtown zones of the Zoning Bylaw.

3.8 Parking and Traffic Calming

Parking downtown must meet the needs of merchants, customers, employees, residents, and visitors. Parking should also be coordinated with the larger goals of enhancing the streetscape, and promoting safety.

The City of Terrace endorses reduced off-street parking requirements for businesses in the downtown study area. The OCP promotes the development of municipal parking facilities on the perimeter of the downtown study area. Such facilities in strategic locations could provide parking within walking distance of all downtown retail and office destinations. This would allow reductions in parking requirements for new development and reduce surface parking in the core. Buildings could occupy more lot area and provide numerous store fronts on the street.

Traffic calming is important in the downtown, as it promotes a more positive pedestrian atmosphere. Long blocks with multiple wide lanes for vehicle travel, such as Lakelse Avenue between Eby and Emerson Streets, and Kalum Street invite driver's to increase their speed and are less attractive for pedestrians. On-street parking is important both for reducing the need for parking lots

and parking structures downtown, but also to help with traffic calming. Cars parked on the street reduce the number of lanes available for vehicle travel and provide a psychological clue to drivers to slow down.

Traffic calming involves both physical and behavioral measures. Physical measures include the following:

Horizontal Deflection – usually involves narrowing park of the street. Sidewalk bulbs, sidewalk extensions, on street parking and traffic circles are all examples.

Vertical Deflection – involves a change in street surface, such as texture, speed bumps, and raised intersection.

Obstructions – involves full and partial road closures, or traffic diverters.

In addition to on-street parking, the existing and proposed streetscape enhancements, such as sidewalk bulbs and mid block pedestrian crossings also promote traffic calming.

Directives

1. The City shall investigate ways to further reduce off street parking requirements by preparing a comprehensive downtown parking plan.

2. The City shall promote the development of coordinated parking structures immediately outside of the downtown core.
3. Parking requirements should be provided on street where possible.
4. Where off-street parking does occur, it shall be located at the side or rear of the building.
5. The City should encourage the use of shared parking lots for adjacent development in the downtown area.
6. The City should identify streets that could benefit from traffic calming through streetscape improvements.



Parking structure with retail at ground level – Kelowna, B.C.

3.9 Signage

Signage is an important element to the urban area and is part of the community image building process. It can tie into the theme of a downtown and provide wayfinding information.

The City of Terrace has begun to implement commonly themed signage on City owned property. The signage promotes the downtown heritage theme by incorporating a base made of river rock and upright wood posts that support the main sign board. Smaller signs and decorative elements are installed below the main board.

The City's decorative signage program could be expanded to incorporate way finding directional signage throughout the **Downtown Plan** area. The directional signage should be oriented towards pedestrians and could be placed at key intersections, trails, and important landmarks. The size of the signs should be such that it is noticeable but not obstructing a large area of the sidewalk, or pathway. Incorporation into nearby landscaping would be preferable.

Directives

1. Signage on City property shall follow the City of Terrace

Signage on City Owned Property Policy (No. 89).

2. Way finding signage should be located throughout downtown at gateways, important community locations, approved buildings, and major intersections.



Way finding signage in downtown Cultural District - Kelowna, B.C.



Signage for the George Little House (top) and the Farmer's Market (lower).

4.0 Implementation Strategy

Action Plan and Priorities

Action	Lead Role	Partners	Funding Source	Priority	Timeframe
Overall Concept Priorities					
1. The historic development pattern of the downtown shall be maintained and enhanced.	City	-	City	HIGH	Ongoing
2. Links between important sites shall be enhanced through streetscape reconstruction.	City		City	HIGH	Ongoing
3. The City should investigate interim solutions for vacant contaminated gas station lots, including discussions with owners to remove fencing and improve aesthetics through plantings and temporary placement of public art or other improvements.	City	Property Owners/ DMA	-	HIGH	Short-term

DOWNTOWN PLAN

Action	Lead Role	Partners	Funding Source	Priority	Timeframe
Theme					
1. The City shall strive to develop a historic theme through maintaining and enhancing the built form of the downtown.	City	DMA/Property Owners/ Dev. Ind.		HIGH	Ongoing
2. Developments and improvements in the Downtown Tourism Area shall fit into the heritage theme and context.	City	Property Owners/Dev. Ind.			Ongoing
3. The City should consider eligibility for placing properties on the community historic registry and the establishment of a Heritage Advisory Commission (HAC).	City	HAC/Historical Society		MEDIUM	Short-term
4. The City shall develop a placard or signage system to identify heritage resources in the downtown.	City	HAC/Historical Society			Mid-term
5. The City should investigate ways to encourage and assist property owners in renewing and revealing the historic architectural elements of the potential heritage buildings.	City	Property Owners/ DMA	Revitalization Tax Area	MEDIUM	Mid-term

DOWNTOWN PLAN

Action	Lead Role	Partners	Funding Source	Priority	Timeframe
Gateways					
1. Gateways to Downtown Terrace shall be constructed to direct traffic to the downtown core, and to reinforce the downtown historic theme.	City	Local Service Clubs/Beautification Society	Grants/Service Clubs	HIGH	Short-term
2. The City should explore options such as grants to provide funding for gateway development.	City			HIGH	Short-term
Public Realm					
Parks					
1. The City shall seek opportunities for upgrading Lower Little Park, such as the Spirit Squares Grant Program.	City	DMA/Beautification Society	Provincial Grants	HIGH	Short-term
2. The City should improve linkages between Upper and Lower Little Parks and investigate the potential for a mid block pedestrian crossing on Kalum Street.	City			Medium	Mid-term
3. The City should investigate the relocation of the skateboard park as part of the park enhancements.	City	Community			Long-term

DOWNTOWN PLAN

Action	Lead Role	Partners	Funding Source	Priority	Timeframe
Streetscape Enhancements					
Street Lighting					
1. New lighting should be installed on all streets within the study area at the time of street reconstruction.	City		City		Ongoing
2. New lighting shall be functional for both automobile scale and the pedestrian scale.	City		City		Ongoing
3. New lighting should reflect the historic theme of the downtown	City		City		Ongoing
Pedestrian Areas					
Sidewalk Treatment					
1. The streetscape reconstruction in the downtown should incorporate widening of sidewalks wherever feasible; in some situations this will include reducing the number of vehicle traffic lanes.	City		City		Ongoing
2. The City should install the autumn and charcoal pavers as the sidewalk design treatment on all pedestrian areas throughout the <i>Downtown Plan</i> area at the same time as future road reconstruction.	City		City		Ongoing

DOWNTOWN PLAN

Action	Lead Role	Partners	Funding Source	Priority	Timeframe
<i>Sidewalk Bulbs and Pedestrian Crossings</i>					
1. The City should install sidewalk bulbs at all downtown intersections at the time of future road reconstruction.	City		City		Ongoing
2. Mid block pedestrian crossings should be incorporated in the design of streets that have a long block length.	City		City		Ongoing
Street Furniture					
<i>Benches</i>					
1. Bench pads should be installed at various downtown locations at the same time as future road construction or during sidewalk reconstruction.	City		City		Ongoing
2. Benches should be installed when funds become available in the park bench donation program account.	City	Community			Ongoing
<i>Waste Receptacles</i>					
1. Waste receptacles should be installed at various downtown locations in accordance with bench placement, concurrent with removal of an older type of existing receptacle, or during future road or sidewalk reconstruction.	City		City		Ongoing

DOWNTOWN PLAN

Action	Lead Role	Partners	Funding Source	Priority	Timeframe
<i>Planters</i>					
1. Sidewalk planting beds should be installed within sidewalk bulbs at intersections in the downtown, at the time of future road reconstruction.	City		City		Ongoing
2. The Beautification Society's portable planters should be relocated to acceptable locations within the downtown or at Ferry Island as street reconstruction occurs, or when hanging baskets can be provided.	Beautification Society	City	Annual Planting Contract		Short-term
3. Hanging baskets should be installed on all reconstructed streets to provide seasonal color and complement the street trees and landscape beds.	Beautification Society	City	Annual Planting Contract		Ongoing
Street Trees, Grates, and Guards					
1. It is recommended that street trees, grates, and guards be installed during future road reconstruction, at the time of adjacent commercial development or as a specific capital project on some downtown streets.	City		City		Ongoing
Bike Racks					
1. Bike racks should be installed at time of future road reconstruction or, if funds permit, at approved locations throughout the downtown.	City	Downtown Merchants	Unbudgeted 2007	HIGH	Ongoing

DOWNTOWN PLAN

Action	Lead Role	Partners	Funding Source	Priority	Timeframe
Beautification and Activity					
1. Hanging baskets should be installed on light poles on a block to block basis where possible rather than the existing street level planters.	DMA/ Beautification Society	City	BIA		Ongoing
2. The City should petition BC Hydro to allow the installation of enhancements.	City	DMA/Beautification Society			Short-term
3. A Banner and decorative lighting program should be initiated.	DMA/Beautification Society	City	BIA		Short-term
4. The City should endorse and support a variety of festivals, activities and events in the downtown.	DMA	City	BIA		Ongoing
Building and Property Improvements					
Old Co-op Site					
1. The planning and development process for the old Co-op site shall involve the community throughout.	City	Dev. Ind./DMA/Community		HIGH	Short-term
2. Development for the site should be consistent with the Vision, referencing the downtown theme.	Dev. Ind.	City	Private Development/ P3	MEDIUM	Short-term
3. Development should incorporate a mix of uses to best achieve the Vision, including a mandatory residential component.	Dev. Ind.	City	Private Development/ P3	MEDIUM	Short-term

DOWNTOWN PLAN

Action	Lead Role	Partners	Funding Source	Priority	Timeframe
Safety, Crime and Vandalism					
1. CPTED guidelines found in Appendix A should be considered for any redevelopments or enhancements done downtown.	City	Dev. Ind.		HIGH	Ongoing
2. Property owners should practice opportunity reduction by not using large rocks, stones, or moveable bricks greater than 2 cm diameter for landscaping and through regular maintenance of property.	Property Owners	City			Ongoing
Downtown Residential and Mixed-Use Development					
1. The City should investigate strategies to stimulate more residential development downtown, including reduced development fees in the <i>Downtown Plan</i> area.	City		Permit Fee Reduction/Revitalization Tax Area		Mid-term
Parking and Traffic Calming					
1. The City shall investigate ways to further reduce off street parking requirements by preparing a comprehensive downtown parking plan.	City			HIGH	Short-term
2. The City should acquire property in suitable locations on the perimeter of the downtown for development as Public Parking Facilities.	City	City	City	HIGH	Short-term

DOWNTOWN PLAN

Action	Lead Role	Partners	Funding Source	Priority	Timeframe
3. The City should encourage the use of shared parking lots for adjacent development in the downtown area.	City	Dev. Ind.			Mid-term
4. The City should identify streets that could benefit from traffic calming through streetscape improvements.	City				Ongoing
Signage					
1. Signage on City property shall follow the City of Terrace Signage on City Owned Property Policy (No. 89).	City				Ongoing
2. Wayfinding signage should be located throughout downtown at gateways, important community locations, approved buildings, and major intersections.	City	DMA		MEDIUM	Short-term
Business Improvement Area					
1. The City shall consult with the Downtown Merchant Association and Chamber of Commerce regarding the establishment of Business Improvement Area in the downtown core.	City	DMA C of Comm.	BIA	HIGH	Short-term

DOWNTOWN PLAN

Action	Lead Role	Partners	Funding Source	Priority	Timeframe
Downtown Revitalization Area					
1. The City should investigate the use of revitalization tax exemptions to promote property new development and improvements to existing commercial property in the downtown.	City	Property Owners/ Dev. Ind./ DMA		MEDIUM	Mid-term

Appendix A - Business Improvement Areas and Downtown Revitalization Areas

1.0 Business Improvement Areas

A Business Improvement Area (BIA) is a limited geographical area designated by bylaw in which businesses and property owners can finance effective marketing, promotional and revitalization programs. It acts as a self-help mechanism designed to assist local business people and property owners promoting their business and shopping district to further trade and commerce in their area. A BIA provides both the organized structure and the source of funds to enable local business communities to improve their commercial viability. A BIA organization coordinates and directly enacts specific activities.

Examples of Business Improvement Areas

Prince George:

- The Business Improvement Area of Downtown Prince George was formed in 1999. A group of businesses and property owners formed the Prince George Downtown Business Improvement Association, which is now known as Downtown Prince George.
- The organization lists its objectives as to organize and manage projects and activities such as streetscape improvements, coordinated marketing programs, planning and economic development initiatives, special events and lobbying.

- Funding for Downtown Prince George comes from an annual levy on DPG area properties of \$1.70 per \$1000 of assessed value.
- There are 209 registered properties and approximately 300 businesses within the business improvement area boundaries.
- The City of Prince George passed a bylaw in May 1999 authorizing the creation of the business association for a five year period from 1999 to 2003. Downtown Prince George received a five year renewal of its mandate in November 2003.

Williams Lake:

- Established in 2004, the Williams Lake Business Improvement Area occupies most of downtown Williams Lake.
- The business association has a 5 year term, which can be renewed.
- Funding comes from a tax levied on businesses in the area of \$0.85 per \$1000 of assessed property value. The total levy collected is \$60,000.

West Quesnel:

- The West Quesnel Business Improvement Association was initiated in 2001 and established the West Quesnel BIA.
- Funding comes from a tax levied on businesses that are inside the Business Improvement Area, totaling an annual levy of \$40,000. The association also earns funds (\$600) from the City of Quesnel for upkeep of a kiosk.
- The organization has completed several projects including: highway signage; welcome signage; an annual country fair; visitor kiosks; regular security patrols; banners; and a website.
- Future plans include: the completion of West Quesnel Visitor Park, which will include a tourist map and heritage information, and paving stones.

Abbotsford:

- Formed in 1989 to focus on improvement of the downtown area. The focus has been on improving the retail area through programs of redevelopment, marketing and beautification.
- Operating budget is \$128,000. with a 3% increase per annum.
- This is collected from property owners through a levy on property taxes. The system used is not efficient because redevelopments and improvements are constantly changing the cost to the taxpayer annually.

The organization would like to switch to a fixed mill rate in order to keep the levy approximately equal for property owners.

- Staff resources of 1½ full time equivalent. One manager and one support staff, with a focus on the accounting.
- Overseen by a Board of Directors.
- Current Programs and budgetary disbursements include:
 - Beautification programs \$6,000
Referred to as the banners, baskets and clean up programs. These have been established to make the downtown look nice and keep it fresh. More appropriate budget level is \$12,000, however due to the tenure of the organization; most of the money is only used for maintenance of existing infrastructure.
 - Advertising programs \$11,000.
Advertising to highlight the downtown merchants.
 - Promotions programs \$8,000.
Used for partnering with other events and programs and providing support to various activities, festivals, etc.
 - Berry Festival funding \$29,000.
A major funding source of the Berry festival, one of Abbotsford's premier annual events.
- A Façade Improvement Grant program is also utilized by the Abbotsford BIA. This program offers property owners grants on façade improvements of \$250/metre for the first 10 metres, reduced to \$225/m after that, to a maximum of \$5000. There are predetermined

guidelines to qualify. This program was initially funded with provincial dollars, which no longer exist. The program is being phased out once funds have been depleted. It was a very usual program to provide a base incentive to the property owners.

Port Coquitlam:

- Formed in 1996, it took two years of organization to develop due to some property owners' uncertainty on the need. The Port Coquitlam BIA (POCOBIA) is finishing their second five year mandate and is going through the renewal process for another five year term.
- The Operating budget is \$150,000, increasing by \$5000/year. Their initial budget was \$40,000.
- Funding mechanism is a levy on assessed value with a floating rate, based on budget.
- Staff resources of ½ full time equivalent, and also rely on volunteers.
- Overseen by a Board of Directors.
- Current Programs and budgetary disbursements (all figures are estimates):
 - Marketing 20%
 - Events 20%
 - Beautification 15%
 - Savings for Capital Projects (ex. Christmas Lights) 30%
 - Operating 15%

Cloverdale:

- Formed in 1995, they are now in their 3rd term.
- The Operating Budget is \$120,000, increasing by \$5,000 a year. The initial budget was \$70,000.
- The funding mechanism is a levy on assessed value, currently the rate is \$1.23/\$1000 assessed value.
- Staff resources of 1 full time equivalent.
- Overseen by a Board of Directors.
- Current Programs and budgetary disbursements (all figures are estimates):
 - Marketing & Events 20%
 - Beautification 10%
 - Safety & Security 10%
 - Communications Committee 10%
 - Savings for Capital Projects (ex. Christmas Lights) 30%
 - Operating 15%

Mission:

- The Mission BIA is currently undergoing staffing changes and was not able to provide significant information, however have provided some basics, as well as a copy of the By-law. They are two years in to their five year mandate.
- The Operating Budget is \$52,500, initial operating budget was \$50,000.
- Staff resources of 1 full time equivalent.
- Overseen by a Board of Directors.

- Current focus is on beautification programs and marketing.

City of Langley:

- Formed in 1997 as a Merchant's Association to look after events. In 1999 it became a BIA, with a five year mandate that has recently been renewed.
- The organization formed in advance of big box development, by the merchants and property owners bought in. The arrival of the big box retailers forced the organization to look at niche marketing, clean up and customer service as driving forces to downtown survival.
- The Operating budget is currently \$180,000. There initial budget was \$124,000. Have expanded the base by expanding the covered area of the by-law rather than increasing the levy.
- Funding mechanism is a levy on assessed value with a floating rate, based on budget.
- Staff resources of 1 full time equivalent, and also rely on volunteers.
- Overseen by a Board of Directors.
- Current Programs and budgetary disbursements (all figures are estimates):
 - Marketing \$10,000
 - Advertising \$35 - 40,000
 - Events \$33,000
 - Operating \$70,000
 - Emergency saving \$5000
 - Security \$3,200

- Larger event support \$7,000
- Beautification was dealt with in one lump sum two years ago, funding by local government and BIA.

2.0 Downtown Revitalization Areas

Downtown Revitalization Area is a limited geographical area where revitalization tax exemptions can occur. Revitalization tax exemptions are a tool that municipalities can use to encourage certain types of property investment and achieve development objectives, including building façade improvements and beautification.

Authority to provide revitalization tax exemptions is contained in Section 226 of the *Community Charter*. The provisions in the charter require that an area must be designated in either the Official Community Plan, or in the municipal financial plan. This designation must include reasons for and objectives of the designation. Along with the designation, a revitalization tax exemption program must be established. The program can include the types of property that are eligible for exemption, the extent or limitations of the exemption, the conditions on which certificates of exemption may be issued, and any provisions for a recapture amount. Revitalization tax exemption certificates can be issued after all the conditions are established by bylaw and the agreement has been met.

Downtown revitalization tax exemption programs are in place in many communities in British Columbia.

Examples of Business Improvement Areas

Abbotsford:

- Revitalization Tax Exemption area for the city's historic downtown (C7 Zone).
- On a trial basis.
- Requires \$100,000 improvement value minimum.
- Five year graduated format.
- 50% commercial content component.

- Impact on City tax revenue is \$14.60/\$1000 of assessed property value.

Prince George:

- Revitalization Tax Exemption area for the Central Business District (C-1 zone)
- 5 year term, with the opportunity for renewal.
- Requires \$50,000 improvement value minimum (construction value).
- An exemption is subject to the maximum of the increase in assessed value between the year before the construction began and the year in which the tax exemption certificate is issued. An exemption of 100% of the increased assessed value will be permitted.
- Impact on City tax revenue is \$10.37/\$1000 of assessed property value for residential, \$20.05/\$1000 for industrial, and \$18.04/\$1000 for business.

Merritt:

- Revitalization Tax Exemption area for the city centre area, which contains both business and residential.
- 5 year term, with the opportunity for renewal.
- Project must comply with design guidelines and meet approval of city centre board.
- If alteration does not require a permit, the tax exemption is \$2000.
- If an alteration requires a building permit, the exemption will be the value of the building permit.

Nanaimo:

- Downtown Residential Conversion Tax Exemption Program has two goals:
 - Create new residential units
 - Preserve heritage buildings
-
- Applies to the list of heritage buildings identified by the City within Heritage Conservation Area #1.
- Non-residential projects also considered if they are heritage buildings and the proposed project is in excess of \$200,000 or 25% of the total pre-renovation value of the property.
- Formal used to determine the length of tax exemption based on the cost of improvements. The term of exemption possible under this program is linked to the cost of seismic, building code, sprinkler, and façade upgrade work required

Parksville:

- Revitalization Tax Exemption area for the city's downtown commercial revitalization area (C-3 zone), as identified in the Bylaw.
- 5 year term
- Requires \$10,000 improvement value minimum (construction value)